



## **PUBLIC NOTICE OF A MEETING**

# **BOARD OF LOCAL IMPROVEMENTS**

**Wednesday, February 14, 2024**

**2:30 P.M.**

**Lorraine H. Morton Civic Center, 2100 Ridge Avenue, Room 2404**

### **AGENDA**

- I.** Call to Order / Declaration of Quorum
- II.** Approval of Meeting Minutes
  - a. October 11, 2023 and November 8, 2023
- III.** Public Comment
- IV.** Alley Projects Update
- V.** Alley Projects Budget Summary
- VI.** New Business
  - a. Petition – Alley N of Main St, E of Fowler Ave
  - b. Petition – Alley N of Payne St, E of Crawford Ave
  - c. Petition – Alley N of Grant St, E of Hartrey Ave
  - d. Special Assessment 1530 – Alley N of Leonard Pl, E of Green Bay Rd
    - i. Approval of Plans and Estimates
    - ii. Schedule Public Hearing
  - e. Special Assessment 1531 – Alley N of Colfax St, E of Bryant Ave
    - i. Approval of Plans and Estimates
    - ii. Schedule Public Hearing
- VII.** Old Business
- VIII.** Roundtable Discussion
- IX.** Adjournment

**NEXT MEETING – March 13, 2024**

*Information about the BLI Committee is available at: [www.cityofevanston.org](http://www.cityofevanston.org). Questions can be directed to Paulina Albazi at 847-866-2950.*

*The City of Evanston is committed to making all public meetings accessible to persons with disabilities. Any citizen needing mobility or communications access assistance should contact 847-866-2919 (Voice) or 847-866-5095 (TTY). Requests for access assistance must be made 48 hours (two working days) in advance. Requests received with less than 48 hours (two working days) advance notice will be attempted using best efforts but cannot be guaranteed.*

## MEETING NOTES

### Board of Local Improvements Wednesday October 11, 2023 2:30 P.M.

Lorraine Morton Civic Center, 2100 Ridge Avenue, Room 2404

Members Present: Edgar Cano, Shawn Pestka, Lara Biggs, Noel Rodriguez, Hitesh Desai  
Members Absent: Clerk Stephanie Mendoza  
Staff Present: Kathryn Loan, Bridget Nash, Paulina Albazi, Kathy Brown  
Presiding Member: Edgar Cano

#### I. Call to Order / Declaration of Quorum

E. Cano calls the meeting to order.

#### II. Approval of Meeting Minutes

##### a. September 13, 2023

L. Biggs makes a motion to approve the minutes from September 13, 2023.

H. Desai seconds the motion. All in favor.

#### III. Public Comment

No Public Present

#### IV. Alley Projects Update

B. Nash provides the board with an update on the alley projects.

The alley North of Washington, east of Sherman is substantially complete.

The CDBG alley North of Linden, East of Asbury, pavement anticipated for next week.

Sewer work has also started in the alley North of Mulford, east of Elmwood.

Work has not yet started in the alley North of Lyons, West of Ashland. This work may have to be pushed into next year.

The alley petition for alley North of Leonard, East of Greenbay has been accepted. The petition for the alley North of Colfax, East of Bryant will be discussed later on the agenda.

Four petitions have been released per last meeting. There are currently 6 active petitions and 72 on the waiting list.

There are currently 77 residents on the wait list to receive petitions.

**V. Alley Project Budget Summary**

B. Nash provides the board with an update on the alley budget summary.

There have been no changes since last meeting. The construction cost paid through September is now included on the 2023 Alley Budget Summary.

**VI. New Business**

B. Nash brings to the board issuance of petitions.

**a. Wesley Avenue North of Channel**

Petitioner lives off Wesley Ave. Lara and Noel have been in contact with the property owner regarding this street. The assessment method is different for streets than that of alleys. The board will need to decide how to assess this before we know how to spread the assessment. In the past, the City would pay two-thirds of the cost and the property owner one pays one-third of the cost. Also, the assessment was spread further down Lincoln Street. The other dilemma is that MWRD owns land along Lincoln Street.

E. Cano states this is classified as a street and should be treated as a street.

L. Biggs makes a motion to release the petition utilizing the one-thirds/two-thirds special assessment method used in past practices for streets by only including the two properties and the City will pay the MWRD cost and on-third cost of the private owners utilizing the units method.

N. Rodriguez seconds the motion. All in favor.

**VII. Old Business**

**a. Alley North of Colfax, East of Bryant**

At the last meeting, the property owners' signature for 2345 Bryant was not verified.

K. Loan from Legal provides the Board with findings that the daughter who signed the petition, based on the property owner's will, daughter Cynthia is the representative of the estate and can sign off on the petition.

L. Biggs makes a motion to accept the petition. S. Pestka seconds the motion. All in favor.

**VIII. Roundtable Discussion**

L. Biggs states two alleys for special assessment were budgeted for the 2024 program. Hitesh had indicated that there is enough of a balance in the special assessment fund to fund these two alleys but after looking more closely into the special assessment fund, it was asked to go back to funding the City's portion out of GO Bonds and the resident

portion through the special assessment fund. The adjustment will be made to the final budget.

**IX. Adjournment**

L. Biggs makes a motion to adjourn. S. Pestka seconds the motion. All in favor.

**NEXT MEETING – November 8, 2023**



## MEETING NOTES

### Board of Local Improvements Wednesday November 8, 2023

2:30 P.M.

Lorraine Morton Civic Center, 2100 Ridge Avenue, Room 2404

Members Present: Shawn Pestka, Lara Biggs, Hitesh Desai, Sat Nagar  
Members Absent: Clerk Stephanie Mendoza, Edgar Cano, Noel Rodriguez  
Staff Present: Kathryn Loan, Bridget Nash, Paulina Albazi, Kathy Brown  
Presiding Member: Lara Biggs

#### **I. Call to Order / Declaration of Quorum**

L. Biggs calls the meeting to order.

#### **II. Approval of Meeting Minutes**

##### **a. October 11, 2023**

Item held to next meeting.

S. Nagar makes a motion to hold item to next meeting. S. Pestka seconds the motion.

All in favor.

#### **III. Public Comment**

No Public Present

#### **IV. Alley Projects Update**

B. Nash provides the board with an update on the current alley projects showing the 2023 project still under construction. Both CDBG alleys are substantially complete along with the Chicago-Main TIF alley. The special assessment alley is almost complete, and the waste transfer station alley should be complete tomorrow.

Currently there are four active alley petitions. The petition for the alley N. of Simpson, W. of Central Park expired on October 26.

The petition for the alley N. of Pyane, E. of Crawford was received. There are now 73 on the waiting list.

#### **V. Alley Project Budget Summary**

B. Nash updates the board that there are no changes to the alley budget summary.

**VI. New Business**

**a. Approve Assessment Method – New Petitions**

**i. Alley North of Lincon, East of Ashland**

This alley petition was previously released in November 2022. The petition sponsor did not circulate the petition but there is another sponsor next on the list. The board had voted to use the unit method of assessment and to assess the apartments on 1500 Central, 1516 Central, and 1520 Central as only half of a unit. Staff recommends using the same assessment method used in 2022 to release this petition.

S. Nagar makes a motion to release the petition. Clerk Mendoza seconds the motion. All in favor.

**VII. Old Business**

**a. Alley North of Lincoln, East of Hartrey**

This petition was released using the unit method of assessment. There are two condo buildings on the eastern end of the alley, 15 units in the northern building and 14 units in the southern building, which are accounting for almost 50% of the vote. Staff is recommending changing the assessment method. B. Nash has spoken with the petition sponsor and board members from both condos.

Four of the property owners in the northern building own garages that have separate P.I.N.s. These will be included in the updated petition.

S. Nagar makes a motion to modify the released petition using a hybrid method of assessing two condo buildings using the linear foot method and the remaining properties using the unit method, to include the four garages.

S. Pestka seconds the motion. All in favor.

**VIII. Roundtable Discussion**

No items to discuss.

**IX. Adjournment**

S. Nagar motions to adjourn. S. Pestka seconds the motion. All in favor.

**NEXT MEETING – December 11, 2023**

## Board of Local Improvements Alley Projects Summary

### 2023 Alleys - Special Assessment

No.	S.A.	Location	Estimated Cost	Status	Accepted by BLI	Public Hearing	Ward
36	1529	North of Chancellor, East of Asbury	\$750,000	Construction Closeout	11/9/2022	2/28/2023	7

Sub-Total (2023 SA Alleys): \$750,000

### 2023 Alleys - CDBG

37	N/A	North of Linden Place, East of Custer	\$220,000	Construction Closeout	N/A		9
38	N/A	North of Mulford, East of Elmwood	\$240,000	Construction Closeout	N/A		8

Sub-Total (2023 CDBG Alleys): \$460,000

### 2023 Alleys - Chicago-Main TIF

39	N/A	North of Washington, East of Sherman	\$310,000	Construction Closeout	N/A		4
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Sub-Total (2023 TIF Alleys): \$310,000

### 2023 Alleys - Waste Transfer Settlement Area

40	N/A	North of Lyons, West of Ashland	\$350,000	Construction Closeout	N/A		5
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Sub-Total (2023 Waste Transfer Alleys): \$350,000

### Potential Alley Paving

No.	S.A.	Location	Estimated Cost	Petition Status	Petition Issued	Expiration	Ward
41	1530	North of Leonard Pl., East of Green Bay Rd.	\$492,720	Petition Accepted - In Design	5/19/2023	8/19/2023	5
42	1531	North of Colfax St., East of Bryant Ave.	\$408,000	Petition Accepted - In Design	5/19/2023	8/19/2023	7
44	-	North of Main St., East of Fowler Ave.	\$528,000	Extension Requested	9/28/2023	12/28/2023	2
45	-	North of Payne St., East of Crawford Ave.	\$220,800	Acceptance Pending	10/5/2023	1/5/2024	6
47	-	North of Grant St., East of Hartrey Ave.	\$336,300	Acceptance Pending	10/9/2023	1/9/2024	7
46	-	North of Lincoln St., East of Hartrey Ave.	\$390,000	Receipt Pending	11/27/2023	2/27/2024	7

<< 2 ACTIVE PETITIONS - 76 RESIDENTS ON WAITING LIST >>

**Board of Local Improvements  
Alley Budget Summary**

**2023 Alleys**

**Estimated Construction Costs**

<b>Estimated Construction Costs</b>				<b>Funding Source</b>			
<b>No.</b>	<b>SA</b>	<b>Location</b>	<b>Total Cost</b>	<b>TIF</b>	<b>SA</b>	<b>CDBG</b>	<b>WTS</b>
36	1529	North of Chancellor, East of Asbury	\$750,000	\$0	\$750,000	\$0	\$0
37	N/A	North of Linden Place, East of Custer	\$220,000	\$0	\$0	\$220,000	\$0
38	N/A	North of Mulford, East of Elmwood	\$240,000	\$0	\$0	\$240,000	\$0
39	N/A	North of Washington, East of Sherman	\$310,000	\$310,000	\$0	\$0	\$0
40	N/A	North of Lyons, West of Ashland	\$350,000	\$0	\$0	\$0	\$350,000
<b>Estimated Totals</b>			<b>\$1,870,000</b>	<b>\$310,000</b>	<b>\$750,000</b>	<b>\$460,000</b>	<b>\$350,000</b>

**Awarded Cost**

<b>Awarded Cost</b>				<b>Funding Source</b>			
<b>No.</b>	<b>SA</b>	<b>Location</b>	<b>Total Cost</b>	<b>TIF</b>	<b>SA</b>	<b>CDBG</b>	<b>WTS</b>
36	1529	North of Chancellor, East of Asbury	\$676,377	\$0	\$676,377	\$0	\$0
37	N/A	North of Linden Place, East of Custer	\$220,000	\$0	\$0	\$220,000	\$0
38	N/A	North of Mulford, East of Elmwood	\$240,000	\$0	\$0	\$240,000	\$0
39	N/A	North of Washington, East of Sherman	\$310,000	\$310,000	\$0	\$0	\$0
40	N/A	North of Lyons, West of Ashland	\$329,212	\$0	\$0	\$0	\$329,212
<b>Awarded Totals (A)</b>			<b>\$1,775,589</b>	<b>\$310,000</b>	<b>\$676,377</b>	<b>\$460,000</b>	<b>\$329,212</b>

**Final Construction Costs**

<b>Final Construction Costs</b>				<b>Funding Source</b>			
<b>No.</b>	<b>SA</b>	<b>Location</b>	<b>Total Cost</b>	<b>TIF</b>	<b>SA</b>	<b>CDBG</b>	<b>WTS</b>
36	1529	North of Chancellor, East of Asbury	\$662,526	\$0	\$662,526	\$0	\$0
37	N/A	North of Linden Place, East of Custer	\$187,036	\$0	\$0	\$187,036	\$0
38	N/A	North of Mulford, East of Elmwood	\$240,000	\$0	\$0	\$240,000	\$0
39	N/A	North of Washington, East of Sherman	\$276,352	\$276,352	\$0	\$0	\$0
40	N/A	North of Lyons, West of Ashland	\$301,482	\$0	\$0	\$0	\$301,482
<b>Pre-Final Totals (B)</b>			<b>\$1,667,397</b>	<b>\$276,352</b>	<b>\$662,526</b>	<b>\$427,036</b>	<b>\$301,482</b>

<b>Surplus (A-B)</b>	<b>\$108,192</b>	<b>\$33,648</b>	<b>\$13,851</b>	<b>\$32,964</b>	<b>\$27,730</b>
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STATE OF ILLINOIS  
COOK COUNTY  
CITY OF EVANSTON  
PLANS FOR  
**PAVING OF ALLEY NORTH OF LEONARD PLACE,  
EAST OF GREEN BAY ROAD**  
SA 1530  
BID #24-XX

INDEX OF SHEETS

- 1 COVER SHEET
- 2 GENERAL NOTES, SUMMARY OF QUANTITIES, AND TYPICAL SECTIONS
- 3-5 ALLEY NORTH OF LEONARD PLACE, EAST OF GREEN BAY ROAD PLAN AND PROFILE
- 6-7 ALLEY NORTH OF LEONARD PLACE, EAST OF GREEN BAY ROAD CROSS SECTIONS

GENERAL NOTES

- ANY REFERENCE TO "STANDARD" THROUGHOUT THE PLANS, SPECIFICATIONS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO THE LATEST STANDARDS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.
- SAWING OF EXISTING SURFACES, WHEN REQUIRED FOR REMOVAL OR CONSTRUCTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE VARIOUS PAY ITEMS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING THE CONSTRUCTION SHALL BE REPAIRED OR REPLACED BY THE THE CONTRACTOR AT HIS OWN EXPENSE.
- THE CONTRACTOR WILL BE REQUIRED TO CONSTRUCT ALL NEW FRAMES AND LIDS IN CONFORMANCE WITH DETAILS OF STANDARD FRAMES AND LIDS SHOWN IN THE PLANS.
- PROTECTIVE COAT WILL BE APPLIED TO CONCRETE CURB AND SIDEWALK. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

NOTE:

- EXISTING CONDITIONS ARE BASED UPON A TOPOGRAPHIC SURVEY. THIS IS NOT A BOUNDARY SURVEY. PROPERTY LINES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY AND SHALL NOT BE USED AS A BASIS FOR CONSTRUCTION LAYOUT.
- CONTRACTOR WILL BE REQUIRED TO ESTABLISH ACTUAL PROPERTY LINES AND CENTER LINE OF ROW FOR PROPOSED IMPROVEMENT, AS DIRECTED BY ENGINEER.
- THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR "CONSTRUCTION AND LAYOUT" PAY ITEM.

**NOTE:**  
THE PROPOSED PROJECT WILL NOT ALTER OR HAVE IMPACT ON THE FLOOD PROTECTION AREAS AND THE PROPOSED DRAINAGE CONDITION OF THE PROJECT SITES.

SIGNED BY: BRIDGET NASH, PE  
SENIOR PROJECT MANAGER



PROJECT LENGTH: 554 FEET (0.105 MILES)

**NOTICE**  
NO WORK TO BE DONE  
WITHIN THE PAVED  
ALLEY AREA FOR FIVE  
YEARS BY ANY UTILITY

FOR UNDERGROUND UTILITY INFORMATION  
AND LOCATIONS, CALL  
**J.U.L.I.E**  
TOLL FREE  
TELEPHONE No. 1-800-892-0123

STANDARD SYMBOLS

→	EXISTING GUY WIRE	→	PROPOSED GUY WIRE	[Pattern]	EX DETECTABLE WARNING
□	EXISTING POWERPOLE	□	PROPOSED POWERPOLE	[Pattern]	PROP DETECTABLE WARNING
⊗	EXISTING STREET LIGHT	⊗	PROPOSED STREET LIGHT	[Pattern]	DRIVEWAY REMOVAL AND REPLACEMENT (CONCRETE)
⊗	EXISTING BUSH	⊗	PROPOSED BUSH	[Pattern]	DRIVEWAY REMOVAL AND REPLACEMENT (ASPHALT)
⊗	EXISTING STUMP	●	PROPOSED CATCH BASIN	[Pattern]	PAVEMENT REMOVAL AND REPLACEMENT
⊗	EXISTING TREE	●	PROPOSED MANHOLE	[Pattern]	PAVEMENT REMOVAL
○	EXISTING CATCH BASIN	○	PROPOSED INLET	[Pattern]	HMA SURFACE REMOVAL
○	EXISTING INLET	○	PROPOSED SANITARY SEWER		
○	EXISTING MANHOLE	○	PROPOSED STORM SEWER		
△	EXISTING CONTROL POINT	○	PROPOSED WATER (SIZE VARIES)		
---	EXISTING CENTERLINE	---	PROPOSED CENTERLINE		
→ 12" CS	EXISTING COMBINED SEWER	→ 12" CS	PROPOSED COMBINED SEWER		
→ 12" SAN	EXISTING SANITARY SEWER	→ 12" SAN	PROPOSED SANITARY SEWER		
→ 12" SS	EXISTING STORM SEWER	→ 12" SS	PROPOSED STORM SEWER		
→ 8" W	EXISTING WATER MAIN	→ 8" W	PROPOSED WATER (SIZE VARIES)		
---	EXISTING FENCE	---	PROPOSED FENCE		
⊥	EXISTING SIGN	⊥	PROPOSED SIGN		
↔	EXISTING SUMMIT	↔	PROPOSED SUMMIT		
⊕	EXISTING VALVE BOX	⊕	CB / MH / VV TO BE FILLED		
⊗	EXISTING CURB TYPE B	⊗	ELEMENT TO BE REMOVED		
⊗	EXISTING CURB AND GUTTER TYPE B6-12	⊗	CB / MH / VV TO ADJUSTED		
		⊗	CB / MH / VV TO RECONSTRUCTED		
		⊗	PROPOSED VALVE BOX		
		⊗	VALVE BOX TO BE ADJUSTED		
		⊗	PROPOSED CURB TYPE B		
		⊗	PROPOSED CURB AND GUTTER TYPE B6-12		

NOTE:

- SEE THE CONTRACT SPECIFICATIONS FOR TYPICAL GENERAL NOTES PERTAINING TO THE REQUIREMENTS AND CONDITIONS OF THE METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL PRIVATE PROPERTIES ABUTTING THE WORK AREA OF THE PROJECT. ANY PRIVATE PROPERTY THAT IS DAMAGED DURING THE CONSTRUCTION PERIOD SHALL BE REPAIRED OR REPLACED TO ITS ORIGINAL CONDITION AT THE CONTRACTORS' EXPENSE. IN ORDER TO DOCUMENT THE ORIGINAL CONDITION OF ALL PRIVATE PROPERTIES ABUTTING THE WORK AREA, PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE PROPERTY OWNERS AND MAKE A VIDEOTAPE RECORD OF THE PROJECT AREA. THIS SHALL INCLUDE THE CONDITION AND IDENTIFICATION (BY ADDRESS) OF ALL FENCES, GARAGES AND GARAGE FLOORS. THIS RECORD SHALL BE USED TO RESOLVE ANY DAMAGE CLAIMS THAT MAY ARISE AFTER THE CONSTRUCTION. A COPY OF THE VIDEOTAPE SHALL BE FURNISHED TO THE RESIDENT ENGINEER PRIOR TO THE START OF CONSTRUCTION.

FLEXIBLE RUBBER CONNECTOR REQUIREMENT

"ALL INLET AND OUTLET PIPES OF SANITARY SEWER MANHOLES AND OTHER UNDERGROUND STRUCTURES, ALL COMBINED AND/OR STORM SEWER MANHOLES, CATCHBASINS, INLETS, AND UNDERGROUND DETENTION STORAGE STRUCTURES, SHALL BE JOINED WITH WATERTIGHT FLEXIBLE RUBBER CONNECTORS CONFORMING TO A.S.T.M. C-443 AND C-923 WITH STAINLESS STEEL BAND."

CITY OF EVANSTON SEWER SYSTEM OWNERSHIP

"ALL COMBINED SEWER AND/OR STORM SEWER SEWER SYSTEM INCLUDING ALL INTERMEDIATE SEWER SYSTEM IS OWNED BY THE CITY OF EVANSTON."

X:\Public Works\Engineering\Engineers\FY 2024 PROJECTS\Alley Improvements\CAD Sheet Files\Special Assessment Sheets\1. COVER - LEONARD GREEN BAY.dwg

	4	
	3	
	2	
	1	
DATE	NO.	REVISION

SCALE	PROJECT NUMBER: SA 1530	ISSUED FOR: REVIEW	DESIGNED BY: PC	SHEET <b>1 OF 7</b>
HORIZONTAL N/A	BID NUMBER: 24-XX	DATE: 02/14/2024	DRAWN BY: BN	
VERTICAL N/A			CHECKED BY: PC, BN	

**GENERAL NOTES**

THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" LATEST EDITION, THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" LATEST EDITION, PROJECT SPECIFICATIONS, ALL APPLICABLE REQUIREMENTS OF THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, THE RESPECTIVE CITY, ALL APPLICABLE REQUIREMENTS OF THE ORDINANCES OF AUTHORITIES HAVING JURISDICTION AND ALL ADDENDA THERETO SHALL GOVERN THIS WORK.

THE STANDARD SPECIFICATIONS, PROJECT SPECIFICATIONS, CONSTRUCTION PLANS AND SUBSEQUENT DETAILS ARE ALL TO BE CONSIDERED AS PART OF THE CONTRACT. INCIDENTAL ITEMS OR ACCESSORIES NECESSARY TO COMPLETE THIS WORK MAY NOT BE SPECIFICALLY NOTED BUT ARE TO BE CONSIDERED A PART OF THE CONTRACT.

THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR MUST VERIFY THE ENGINEER'S LINE AND GRADES. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTION FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSION OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTION, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.

BEFORE ACCEPTANCE BY THE OWNER AND FINAL PAYMENT, ALL WORK SHALL BE INSPECTED AND APPROVED BY THE OWNER OR HIS REPRESENTATIVES. FINAL PAYMENT WILL BE MADE AFTER ALL OF THE CONTRACTOR'S WORK HAS BEEN APPROVED AND ACCEPTED. WHENEVER DURING CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF GUTTERS, DRAINAGE STRUCTURES, DITCHES, ETC., SUCH THAT THE NATURAL FLOW LINE OF WATER IS OBSTRUCTED, THE LOOSE MATERIAL WILL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES AND FLOW LINES SHALL BE FREE FROM DIRT AND DEBRIS. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR'S FAILURE TO PROVIDE THE ABOVE WILL PRECLUDE ANY POSSIBLE ADDED COMPENSATION REQUESTED DUE TO DELAYS OR UNSUITABLE MATERIALS CREATED AS A RESULT THEREOF.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SIGNS, TRAFFIC CONTROL DEVICES, AND WARNING DEVICES TO INFORM AND PROTECT THE PUBLIC DURING ALL PHASES OF CONSTRUCTION.

WHENEVER THE PERFORMANCE OF WORK IS INDICATED ON THE PLANS AND NO ITEM IS INCLUDED IN THE CONTRACT FOR PAYMENT, THE WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. ADDITIONAL COMPENSATION WILL BE ALLOWED.

DURING CONSTRUCTION OPERATIONS THE CONTRACTOR SHALL ENSURE POSITIVE SITE DRAINAGE AT THE CONCLUSION OF EACH DAY. SITE DRAINAGE MAY BE ACHIEVED BY DITCHING, PUMPING OR ANY OTHER METHOD ACCEPTABLE TO THE ENGINEER AND RESPECTIVE CITY. THE CONTRACTOR'S FAILURE TO PROVIDE THE ABOVE WILL PRECLUDE ANY POSSIBLE ADDED COMPENSATION REQUESTED DUE TO DELAYS OR UNSUITABLE MATERIALS CREATED AS A RESULT THEREOF.

ALL CONSTRUCTION WILL BE INSPECTED BY THE ENGINEER AND THE CITY. SPECIFICALLY ALL TRENCHES AND SEWERS SHALL BE LEFT OPEN (BUT SAFELY BARRICADED) UNTIL INSPECTED AND APPROVED BY THE CITY ENGINEER. PAVEMENT SUBBASE, BASE, AND SURFACE MUST BE EACH BE INSPECTED BY THE ENGINEER AND THE RESPECTIVE CITY PRIOR TO THE NEXT PHASE OF WORK. PROOF ROLLING AND NUCLEAR DENSITY TESTING WILL BE UTILIZED.

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REMOVE FROM THE SITE ANY AND ALL MATERIALS AND DEBRIS WHICH RESULT FROM HIS CONSTRUCTION OPERATIONS AT NO ADDITIONAL EXPENSE TO THE OWNER.

WHEN A CONFLICT BETWEEN PLANS AND SPECIFICATIONS OR NOTES OCCURS, THE ENGINEER SHALL DECIDE WHICH GOVERNS. GENERALLY, THE MORE RESTRICTIVE, MORE SPECIFIC, OR STRICTER PROVISION SHALL GOVERN.

CONTRACTOR IS RESPONSIBLE FOR RETURNING ALL AREAS AFFECTED BY EQUIPMENT OR LABORERS TO EXISTING CONDITIONS. CONTRACTOR IS ALSO RESPONSIBLE FOR PROTECTING ALL NEW WORK UNTIL COMPLETION OF THIS CONTRACT.

THE CONTRACTOR SHALL INDEMNIFY THE OWNER, THE ENGINEER, THE RESPECTIVE CITY AND THEIR AGENTS, FROM SIXTY LIABILITY INVOLVED IN THE CONSTRUCTION, INSTALLATION AND TESTING OF THE WORK ON THIS PROJECT. THE CONTRACTOR MUST CARRY INSURANCE IN ACCORDANCE WITH CITY SPECIFICATIONS AND MUST PRESENT A JOB-SPECIFIC CERTIFICATE OF INSURANCE TO THE ENGINEER AND EMPLOYEES OF THE OWNER, THE CITY, AND THE ENGINEER, AS ADDITIONAL INSURED.

EXISTING UTILITIES: WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF THE ENGINEER AS TO THE LOCATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE CONTRACTOR. THE ENGINEER AND OWNER ASSUME NO RESPONSIBILITY WHATSOEVER IN RESPECT TO THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS RELATIVE TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES OR THE MANNER IN WHICH THEY ARE TO BE REMOVED OR ADJUSTED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES. HE SHALL ALSO OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES, JULIE, THE RESPECTIVE CITY, DETAILED INFORMATION RELATIVE TO THE LOCATION OF THEIR UTILITIES AND THE WORKING SCHEDULES OF THE UTILITY COMPANIES FOR REMOVING OR ADJUSTING THEM.

ALL WORK PERFORMED UNDER THIS CONTRACT SHALL BE GUARANTEED BY THE CONTRACTOR AND HIS SURETY FOR A PERIOD OF 12 MONTHS FROM THE DATE OF INITIAL ACCEPTANCE OF THE WORK BY THE OWNER AGAINST ALL DEFECTS IN MATERIALS AND WORKMANSHIP OF WHATEVER NATURE.

REMOVED PAVEMENT, SIDEWALK, CURB AND GUTTER, ETC. SHALL BE DISPOSED OFF SITE AT LOCATIONS PROVIDED BY THE CONTRACTOR AT HIS EXPENSE.

CONSTRUCTION MEANS, METHODS, AND JOBSITE SAFETY ARE THE SOLE AND EXCLUSIVE RESPONSIBILITY OF THE CONTRACTOR.

CONSTRUCTION ACTIVITIES MAY OCCUR BETWEEN 7:00 AM AND 5:00 PM MONDAY THROUGH FRIDAY AND 8:00 AM TO 4:00 PM ON SATURDAY (AS APPROVED BY THE ENGINEER). CONSTRUCTION ACTIVITIES ON SUNDAY ARE PROHIBITED. NO WORK WILL BE PERFORMED ON STATE OF ILLINOIS OBSERVED HOLIDAYS. ANY VIOLATION FOR WORKING HOURS CONTRACTOR WILL BE FINED \$500.00 FOR OCCURRENCE.

**PAVING AND GRADING NOTES**

ALL PAVEMENT DIMENSIONS ARE TO THE EDGE OF PAVEMENT OR FACE OF CURB, UNLESS OTHERWISE INDICATED. CURB RADI ARE TO THE BACK OF CURB UNLESS OTHERWISE INDICATED.

PAVING WORK SHALL INCLUDE FINAL SUB-GRADE SHAPING AND PREPARATION, FORMING, PLACEMENT OF BASE COURSE MATERIALS, AND SUBSEQUENT BINDER AND/OR SURFACE COURSES, FINISHING AND CURING OF CONCRETE, FINAL CLEAN-UP AND ALL RELATED WORK.

THE PROPOSED PAVEMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.

PAVEMENT DESIGN SHALL BE AS DETAILED, PRIOR TO THE PLACEMENT OF ANY STONE BASE ALL SUBBASE SHALL BE COMPACTED WITH A SELF-PROPELLED SHEEPSFOOT COMPACTOR (CAT #15 OR LARGER) TO A MINIMUM DENSITY OF 95% MOISTURE PROCTOR. TESTING SHALL BE BY NUCLEAR DENSITY TEST AND PROOF ROLLING. THE CONTRACTOR SHALL PROOF ROLL THE SUBGRADE BEFORE PAVEMENT CONSTRUCTION PROCEEDS. THE PROOF ROLL MUST BE WITNESSED BY THE ENGINEER OR HIS REPRESENTATIVE. THE SUBGRADE WILL NOT BE APPROVED AND ACCEPTED WITHOUT PROOF ROLLING. THE PROOF ROLLING SHALL BE DONE BY A FULLY LOADED, THREE-AXLE DUMP TRUCK TOGETHER WITH LOAD WEIGHING AT LEAST TWENTY FIVE (25) TONS. THE SUBGRADE OR BASE HAS FAILURE OR PUMPING AS INDICATED BY PROOF ROLLING. THE AREA OF FAILURE OR PUMPING SHALL BE SCARIFIED AND RECOMPACTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. SUBSEQUENT PROOF ROLL WILL BE CONDUCTED UNTIL THE SUBGRADE IS FOUND TO BE ACCEPTABLE TO THE ENGINEER.

A MODIFIED PROCTOR DENSITY OF 95% IS REQUIRED FOR STONE BASE, A DENSITY OF 93% IS REQUIRED FOR ASPHALT. ALL OFF-ROAD ZONES SHALL BE COMPACTED TO A MINIMUM OF 90% STANDARD PROCTOR. FILL CANNOT INCLUDE DEBRIS. (REMOVE ALL DEBRIS, TREES, ETC. FROM SITE).

ALL DISTURBED NON-PAVEMENT AREAS SHALL BE ROUGH GRADED. THE CONTRACTOR IS RESPONSIBLE FOR ALL EROSION PREVENTION AND REPAIR.

CURB AND GUTTER SHALL BE 6.12 VERTICAL CURB OR MATCH EXISTING FOR SHORT SECTIONS. THE CURB SHALL CONSIST OF PORTLAND CEMENT CONCRETE, 4" SLUMP, 6.1 BAG MIX, AND AIR ENTRAINMENT OF NOT LESS THAN FIVE (5%) OR MORE THAN EIGHT (8%). CONCRETE SHALL BE MINIMUM COMPRESSIVE STRENGTH (3000 PSI) AT FOURTEEN (14) DAYS. ALL CURB AND GUTTER SHALL BE BROOM FINISHED. SAW CONTRACTION JOINTS AT LEAST 2" IN DEPTH 15' O.C. AND AT ALL POSTS & FITS WITHIN 24 HOURS. CURB AND GUTTER SHALL BE BACKFILLED AFTER PAVEMENT IS INSTALLED. IT SHALL BE BACKFILLED AND COMPACTED FOR A DISTANCE OF 5 FEET AT A 2% GRADE.

3/4" THICK PRE-MOLDED FIBER EXPANSION JOINTS WITH 3/4" x 18" PLAIN ROUND STEEL DOWEL BARS SHALL BE INSTALLED IN ALL CURBS, 1/2" x 12" PLAIN ROUND STEEL DOWEL BARS IN SIDEWALK AT 100' SIXTY (60) FEET INTERVALS AND AT ALL T.C.'S, P.T.'S. CURB RETURNING SHALL BE ALTERNATE ENDS OF THE DOWEL BARS SHALL BE GREASED AND FITTED WITH METAL EXPANSION TUBES. ALL EXPANSION JOINTS MUST BE FREE OF CONCRETE FOR FULL DEPTH.

CONTRACTION JOINTS SHALL BE TOoled AT 5' INTERVALS IN THE SIDEWALK. THE COST OF THESE JOINTS SHALL BE CONSIDERED AS INCIDENTAL TO THE COST OF THE CONTRACT. CONTRACTION JOINTS SHALL ALSO BE TOoled DOWN THE CENTER OF ALL SIDEWALKS GREATER THAN 6' WIDE. (FIVE FOOT SPACING MAXIMUM).

ALL PORTLAND CEMENT CONCRETE SHALL BE CURED AND PROTECTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.

CONCRETE SURFACE (SIDEWALKS & DRIVEWAY PAVEMENTS)

A) FINISH WITH A LIGHT BROOM FINISH.

B) 6.1 BAG MIX, 4" SLUMP MAX. 5%-8% AIR ENTRAINMENT. READY-MIX CONCRETE.

C) 3500 PSI AT 14 DAYS, TWO CYLINDERS/60 CY.

D) SLOPE AT MINIMUM 1%, OR AS NOTED ON PLANS.

WHENEVER NEW CONCRETE ABUTS EXISTING CONCRETE, SET A 3/4" THICK PRE-MOLDED FIBER EXPANSION JOINT AND 3/4" IDOT STANDARD EXPANSION ANCHOR TIES IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. THIS INCLUDES CONCRETE POURED ADJACENT TO EXISTING SIDEWALKS, CURBS AND BUILDINGS. THE DOWEL BARS SHOULD BE 4" INTO EXISTING CONCRETE WITH 8" EXTENDING INTO NEW CONCRETE.

PRIME COAT FOR THE SURFACE COURSE SHALL BE APPLIED TO THE BINDER AT A RATE OF 0.07 GAL/SY (SS-1) PRIME COAT FOR THE BINDER COURSE SHALL BE APPLIED TO THE SUBBASE AT A RATE OF 0.25 GAL/SY (P.E.P.) PRIME COAT WILL NOT BE PAID FOR SEPARATELY.

WHEREVER A PAVEMENT OVERLAY OCCURS ADJACENT TO A CONCRETE SURFACE OR EXISTING PAVEMENT, THE CONTRACTOR SHALL GRIND DOWN (OR REMOVE) ASPHALT 3" WIDE BY 1.5" DEPTH UNLESS OTHERWISE NOTED ON THE PLANS. THIS IS CONSIDERED INCIDENTAL TO THE CONTRACT.

ALL CURB AND SIDEWALK SHALL BE REINFORCED WITH TWO #4 REBARS (THREE EQUALLY SPACED REBAR FOR SIDEWALK) WHENEVER THE CURB OR SIDEWALK CROSSES A UTILITY TRENCH. EXTEND THE REBAR TEN FEET BEYOND THE TRENCH ON BOTH SIDES.

ALL PAVEMENT MARKING SHALL BE EPOXY PAVEMENT MARKINGS.

ALL SIGN POSTS SHALL CONFORM TO IDOT AND MUTCD STANDARDS.

**PORTLAND CEMENT CONCRETE PAVEMENT**

**GENERAL**

PORTLAND CEMENT CONCRETE PAVEMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 420 OF THE "STANDARD SPECIFICATIONS", EXCEPT THAT THE FINAL FINISH SHALL BE OBTAINED BY THE USE OF TWO (2) SEPARATE BURLAP DRAGS HAVING A DOUBLE THICKNESS AT LEAST FOUR (4) FEET WIDE AND TWO (2) FEET LONGER THAN WIDTH OF SLAB UNDER CONSTRUCTION. THE BURLAP SHALL BE ATTACHED TO A BRIDGE RIDING ON PAVEMENT FORMS AND BE KEPT SATURATED WHILE IN USE. IT SHALL BE LAID ON THE PAVEMENT SURFACE AND DRAGGED IN THE DIRECTION THE PAVEMENT IS BEING LAID, WITH APPROXIMATELY TWO (2) FEET OF WIDTH IN CONTACT WITH THE PAVEMENT SURFACE. THE BURLAP DRAG SHALL BE KEPT CLEANED AND FREE OF HARDENED CONCRETE.

THE ENGINEER MAY REQUIRE CHANGES IN BELTING, BURLAPPING AND EQUIPMENT TO PRODUCE DESIRED FINAL SURFACE TEXTURE.

**PREPARATION BASE**

ALL LABOR, EQUIPMENT AND MATERIAL NECESSARY FOR BASE PREPARATION SHALL BE INCLUDED IN THE VARIOUS PAY ITEMS. THIS WORK WILL NOT BE PAID FOR SEPARATELY.

**TRANSVERSE EXPANSION JOINTS**

TRANSVERSE EXPANSION JOINTS SHALL BE PLACED THROUGHOUT THE ENTIRE IMPROVEMENT ACROSS THE CONCRETE PAVEMENT AT RIGHT ANGLES TO THE EDGE THEREOF AND AT INTERVALS OF SIXTY FEET (60') MEASURED ALONG SAID CENTER LINE. THE EXPANSION JOINTS SHALL BE MADE BY PLACING AND LEAVING IN PLACE PRIOR TO LAYING THE CONCRETE MIXTURE FOR THE PAVEMENT, BITUMINOUS PRE-MOLDED JOINT FILLER CONFORMING TO THE REQUIREMENTS OF ARTICLE 751.03 OF THE "STANDARD SPECIFICATIONS". SAID JOINT FILLER SHALL BE ONE INCH (1") IN THICKNESS AND SHALL EXTEND VERTICALLY FROM A POINT ONE INCH (1") BELOW THE TOP OF THE SUBGRADE TO A POINT ONE-HALF INCH (1/2") BELOW THE FINISHED GRADE OF THE PAVEMENT. THE INTERVENING SPACE BETWEEN THE TOP OF SAID JOINT FILLER AND THE FINISHED GRADE OF THE PAVEMENT SHALL BE FILLED WITH ASPHALT P.F.A.-3.

LOAD TRANSFER ACROSS SAID EXPANSION JOINT SHALL BE ACCOMPLISHED BY INSTALLATION OF ONE INCH (1") ROUND, PRE-COATED, SMOOTH STEEL BARS EIGHTEEN INCHES (18") LONG AT TWELVE INCH (12") CENTER THROUGH SAID JOINT FILLER. SAID STEEL BARS SHALL BE APPROVED BY THE ENGINEER. ONE END OF EACH OF SAID STEEL BAR SHALL BE FURNISHED WITH A METAL OR FIBER CAP TO PROVIDE ONE INCH (1") OF EXPANSION SPACE. SAID STEEL BARS SHALL BE MAINTAINED RIGIDLY IN PROPER ALIGNMENT BY APPROVED MEANS.

ALL MAN-HOLE FRAMES AND OTHER FIXED OBJECTS, WITH THE EXPANSION OF FRAMES OTHERWISE MENTIONED IN THESE SPECIFICATIONS, SHALL BE SEPARATED FROM THE CONCRETE PAVEMENT BY JOINT FILLER OF THE SAME THICKNESS AND CHARACTER AS HEREBEFORE SPECIFIED FOR EXPANSION JOINTS. WHICH JOINT FILLER SHALL EXTEND THROUGH THE ENTIRE THICKNESS OF THE PAVEMENT. SAID JOINT FILLER SHALL BE PLACED IN SUCH A MANNER AS TO FORM A SQUARE WHOSE SIDES WILL BE FIFTEEN INCHES (15") FROM THE UPPER OUTSIDE EDGES OF THE FRAMES.

FURNISHING ALL LABOR, EQUIPMENT, AND MATERIAL FOR INSTALLATION OF SAID EXPANSION JOINTS, LOAD TRANSFER DEVICES AND REINFORCING STEEL SHALL NOT BE PAID FOR DIRECTLY, BUT WILL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION OF THE PORTLAND CEMENT CONCRETE PAVEMENT.

CONTRACTION JOINT

SEE THE DETAIL FOR CONTRACTION JOINT. ALL SAW JOINT TO BE FILLED FLUSH WITH SEALER.

**MIXING AND PLACING CONCRETE**

CONCRETE SHALL BE MIXED AND PLACED IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 420 OF THE "STANDARD SPECIFICATIONS". INSURE THAT THE SAME MAY APPLY, WHILE BEING PLACED, THE CONCRETE SHALL BE VIGOROUSLY VIBRATED BY THE USE OF A MECHANICAL VIBRATING DEVICE, AND SPADED TO ELIMINATE VOIDS OR HONEYCOMB POCKETS AND BULKHEADS. THE CONCRETE SHALL BE STRUCK OFF, SO THAT THE UPPER SURFACE SHALL CONFORM TO AND BE SUFFICIENTLY HIGHER THAN THE CONTOUR OF THE UPPER SURFACE OF THE FINISHED PAVEMENT, TO GIVE THE SPECIFIED THICKNESS OF PAVEMENT HEREIN REQUIRED. THE STRIKE BOARD SHALL BE MOVED FORWARD WITH A COMBINED LONGITUDINAL AND TRANSVERSE MOTION, AND SO MANIPULATED AS TO TAMP THE CONCRETE THOROUGHLY. A SLIGHT EXCESS OF MATERIAL SHALL BE KEPT IN FRONT OF THE CUTTING EDGE AT ALL TIMES. THE ENTIRE AREA OF THE PAVEMENT SHALL BE STRUCK OFF AND TAMPED, SO EXECUTED AS TO INSURE MAXIMUM COMPACTION.

**FINISHED CONCRETE**

AFTER THE VIBRATING AND THE TAMPING HAS BEEN COMPLETED, THE PAVEMENT SHALL BE RUBBED SMOOTH OF ALL RIDGES AND DEPRESSIONS AND BROUGHT TO A TRUE EVEN SURFACE BY THE USE OF WOODEN FLOATS, A SPLIT FLOAT, SHAPED ON THE UNDER SIDE TO CONFORM THE CONTOUR OF THE PAVEMENT, SHALL BE USED FOR FINISHING THE CONCRETE ON BOTH SIDES OF THE LONGITUDINAL JOINTS, HEREBEFORE SPECIFIED. THE EDGES ADJOINING THE TRANSVERSE EXPANSION JOINT AND THE FORM SHALL BE TOoled WITH AN EDGING TOOL HAVE A RADIUS OF ONE-QUARTER INCH (3/4").

AFTER THE FLOATING HAS BEEN COMPLETED, THE SURFACE OF THE PAVEMENT SHALL BE TESTED WITH A NOTCHED STRAIGHT EDGE, TEN FEET (10') IN LENGTH, THIS STRAIGHT EDGE SHALL BE PLACED OVER THE TRANSVERSE JOINTS AND BETWEEN THE SAME AT FREQUENT INTERVALS, PARALLEL TO THE CENTER LINE OF THE PAVEMENT AND MOVED ACROSS FROM SIDE TO SIDE. ANY VIBRATION IN EXCESS OF ONE-EIGHTH INCH (1/8") ABOVE OR BELOW THE GENERAL ESTABLISHED CONTOUR OF THE PAVEMENT SHALL BE RUBBED WITH A LONG HANDLED FLOAT AND THE IRREGULARITIES CORRECTED UNTIL THE STRAIGHT EDGE SHALL SHOW A BEARING FOR ITS ENTIRE LENGTH.

WORKMEN SHALL NOT WALK ON FRESHLY LAID CONCRETE AND NO ONE SHALL BE ALLOWED ON THE CONCRETE AFTER IT HAS BEEN WORKED INTO FORM AND SHAPE UNTIL IT HAS ENTIRELY HARDENED. THE ENTIRE SURFACING OF THE CONCRETE PAVEMENT SHALL BE DONE WITH A BRIDGE, NOT LESS THAN ONE FOOT (1') IN WIDTH WHICH SHALL NOT COME IN CONTACT WITH THE CONCRETE PAVEMENT AT ANY POINT. TWO (2) OF SAID BRIDGES SHALL BE MAINTAINED IN CONSTANT READINESS TO BE IMMEDIATELY MOVED INTO POSITION, TO PROVIDE ACCESS FOR FINISHING, TOOLING AND EDGING JOINTS, ADJUSTING AND REFINISHING DAMAGED PORTIONS OF THE FINISHED SURFACE.

**PROTECTION AND CURING**

PROTECTION AND CURING SHALL COMPLY WITH THE REQUIREMENTS OF SECTION 420 OF THE "STANDARD SPECIFICATIONS".

**PROTECTIVE SEALER**

TO REDUCE THE PENETRATION OF SALT, THE NUMBER ONE CAUSE OF REPAIR CORROSION, SPALLING AND CRACKING, A PROTECTIVE SURFACE TREATMENT CONSISTING OF BSM 20 OR EQUAL SHALL BE APPLIED TO THE SURFACE OF THE PORTLAND CEMENT CONCRETE PAVEMENT. BEFORE THE SURFACE TREATMENT MIXTURE IS APPLIED, THE CONCRETE SURFACE SHALL HAVE A MINIMUM CURING PERIOD OF 28 CALENDAR DAYS PRIOR TO THE APPLICATION OF THE MIXTURE AND SHALL BE CLEANED TO REMOVE ALL OIL, GRIME, AND LOOSE PARTICLES WHICH WOULD PREVENT THE MIXTURE FROM PENETRATING THE CONCRETE. IF CHEM-TRETE BSM 20 WILL BE USED FOR PROTECTIVE SURFACE TREATMENT, SODIUM SILICATE CURING COMPOUND SHALL BE APPLIED FOR CONCRETE CURING, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE TEMPERATURE OF CONCRETE AND THE AIR SHALL BE 50 DEG. F OR HIGHER AT THE TIME OF APPLICATION.

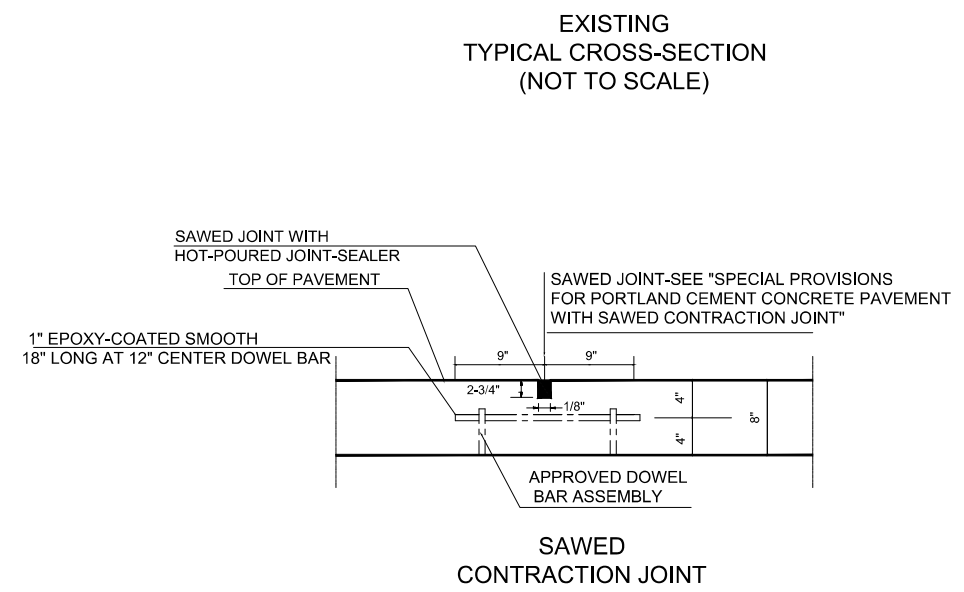
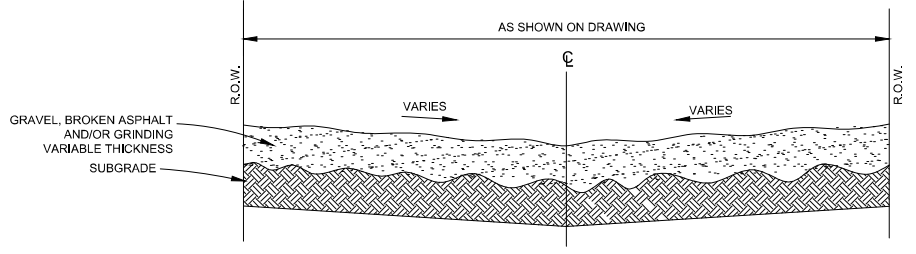
## SUMMARY OF QUANTITIES

### PROJECT NO.: SA 1530

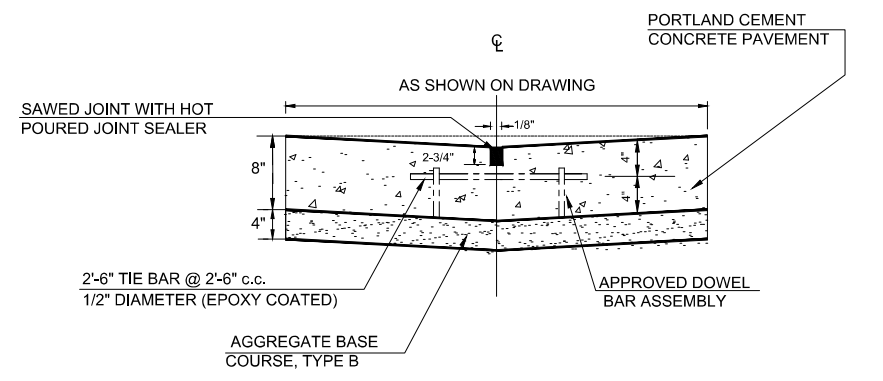
### PAVING OF ALLEY NORTH OF LEONARD PLACE, EAST OF GREEN BAY ROAD

ITEM NO.	ITEM	UNIT	QUANTITY
1	EARTH EXCAVATION	CJ. YD.	583
2	TRENCH BACKFILL	CJ. YD.	439
3	SUB-BASE GRANULAR MATERIAL, TYPE B	CJ. YD.	15
4	GRANULAR CRADLE MATERIAL, CA-11	TON	296
5	AGGREGATE BASE COURSE, TYPE B, 4"	SQ. YD.	1,395
6	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	6
7	PORTLAND CEMENT CONCRETE PAVEMENT, 8'	SQ. YD.	1,385
8	STORM SEWERS, SPECIAL, 10" (D. I. P. / CL 50)	FOOT	162
9	STORM SEWERS, 12", TYPE 2 (R. C. P. / CL III)	FOOT	624
10	STORM SEWERS, SPECIAL 12", (D. I. P. / CL 50)	FOOT	8
11	CATCH BASIN, TYPE A, 4' DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1
12	MAN-HOLE, TYPE A, 4' DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	4
13	INLET, TYPE A, TYPE 1 FRAME, OPEN LD	EACH	4
14	COMBINATION CONCRETE CURB AND GUTTER, TYPE B 6.12	FOOT	90
15	CONCRETE CURB, TYPE B	FOOT	175
16	PAVEMENT REMOVAL	SQ. YD.	45
17	DRIVEWAY PAVEMENT REMOVAL	SQ. YD.	400
18	CURB REMOVAL	FOOT	265
19	SIDEWALK REMOVAL	SQ. FT.	310
20	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT - 6"	SQ. YD.	400
21	PORTLAND CEMENT CONCRETE SIDEWALK - 5"	SQ. FT.	310
22	PROTECTIVE SEALER	SQ. YD.	1,785
23	CRUSHED STONE, CA - 14	TON	30
24	FURNISHING AND PLACING TOPSOIL, 3"	SQ. YD.	140
25	SODDING, SALT TOLERANT	SQ. YD.	140
26	DETECTABLE WARNINGS	SQ. FT.	20
27	TREE REMOVAL (6 TO 15 INCH DIA.)	UNIT DIA.	11
28	HOT-MIX ASPHALT SURFACE REMOVAL	SQ. YD.	35
29	TEMPORARY FENCE	FOOT	140
30	CONSTRUCTION LAYOUT AND STAKING	LUMP SUM	1
31	STREET SWEEPING	EACH	3
32	CLOSED CIRCUIT TV INSPECTION	LUMP SUM	1
33	PRE-CONSTRUCTION SURFACE VIDEO TAPING	LUMP SUM	1
34	REMOVING AND RESETTING PAVERS	SQ. FT.	85
35	EPOXY PAVEMENT MARKING LINE- 6" YELLOW	FOOT	125
36	SOIL DISPOSAL ANALYSIS	EACH	1
37	NON SPECIAL WASTE DISPOSAL	CJ. YD.	111

NO.	DESCRIPTION	ELEV.
OSBM	NORTH SIDE OF BOTTOM STEP TO HOUSE	19.61
23-20	# 2202 WESLEY AVENUE	20.81
OSBM	SPIKE IN 1ST UTILITY POLE EAST OF WEST	20.81
23-21	END OF ALLEY	18.44
OSBM	SQUARE CUT ON LIGHT POLE BASE AT THE	18.44
23-22	SOUTHEAST CORNER OF NORTH-SOUTH ALLEY & NOYES STREET	



ALL SAWED JOINT TO BE FILLED FLUSH WITH SEALER, HOT Poured TYPE FOR JOINTS IN CONCRETE S.S.-5-164, OR WITH COLD APPLIED READY MIXED CONCRETE JOINT SEALING COMPOUND AS SPECIFIED IN THE "SPECIAL PROVISION FOR PORTLAND CEMENT CONCRETE PAVEMENT WITH SAWED CONTRACTION JOINTS"



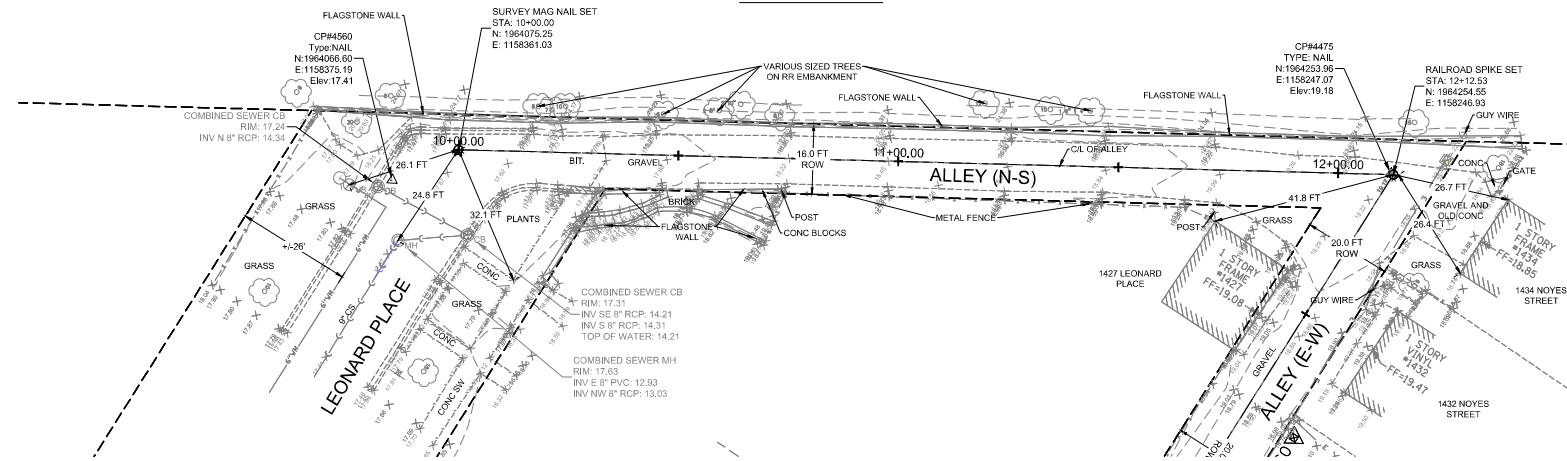
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# ALLEY N OF LEONARD PL, E OF GREEN BAY RD

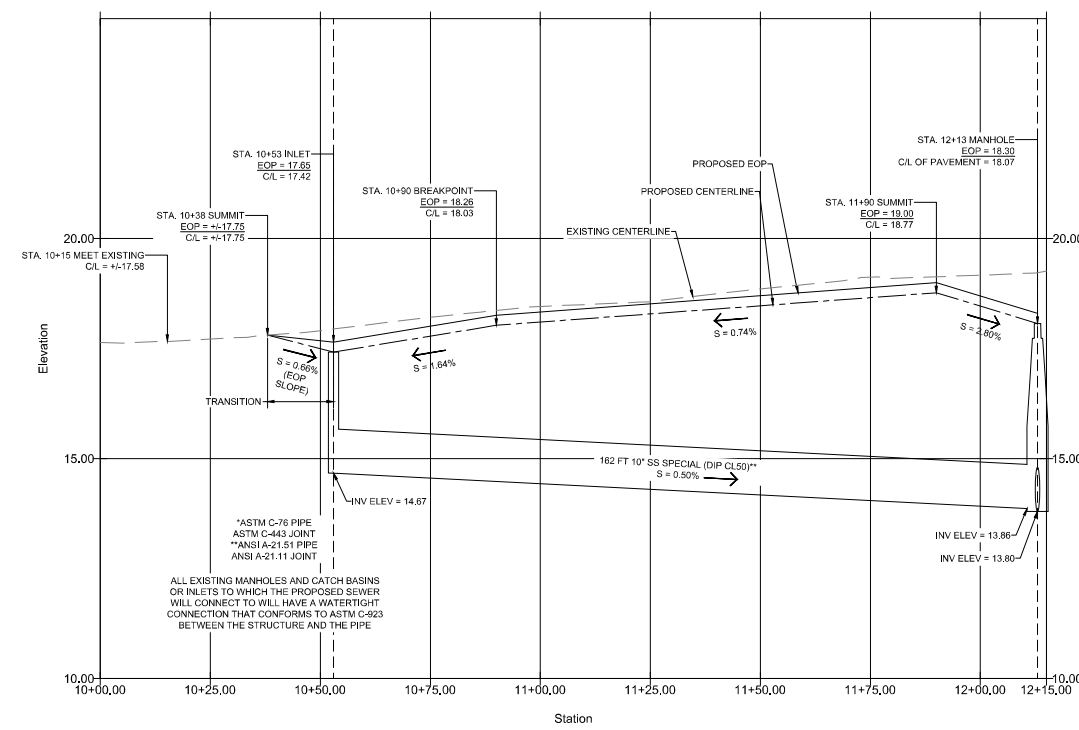
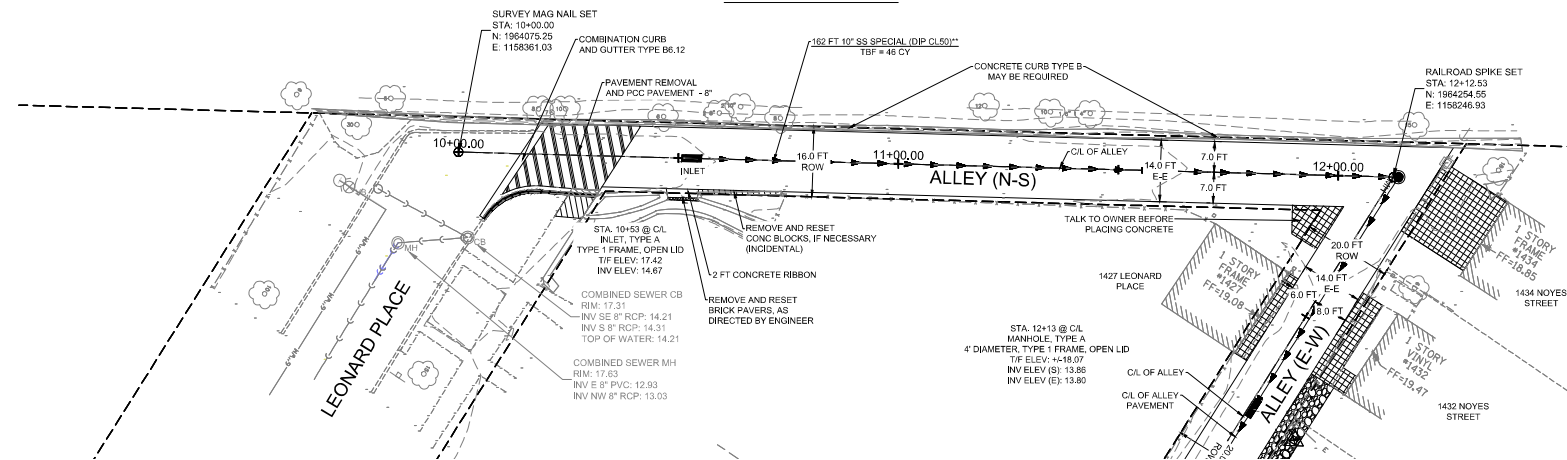
## GENERAL NOTES, SOQ, AND TYPICAL SECTIONS



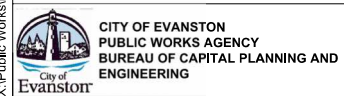
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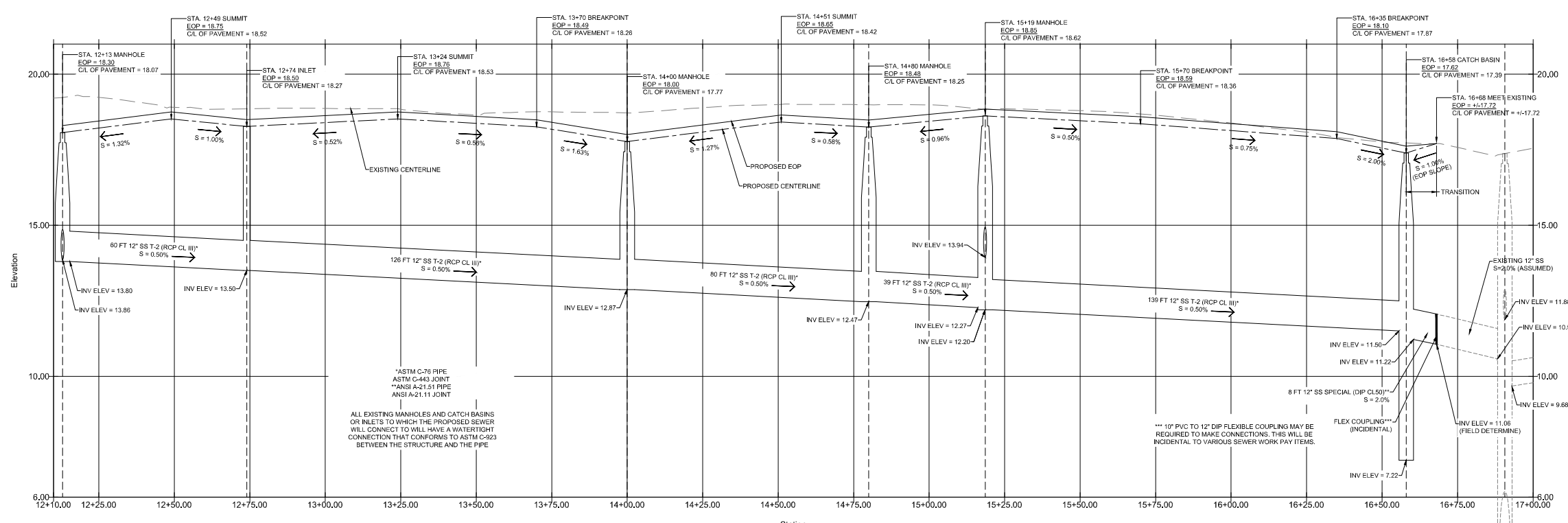
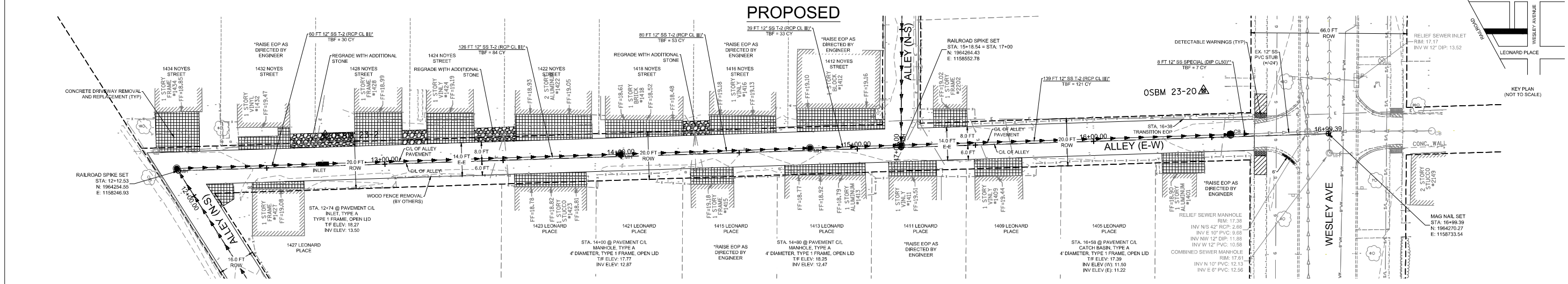
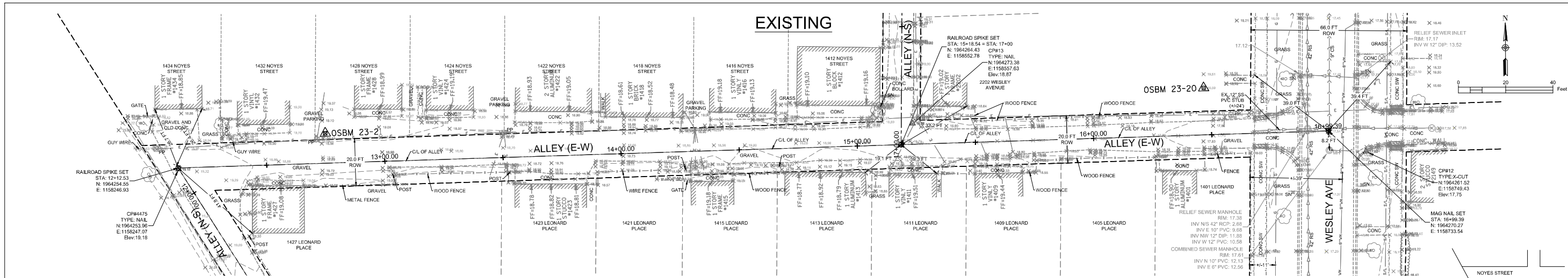
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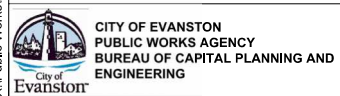
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**ALLEY N OF LEONARD PL, E OF GREEN BAY RD  
PLAN AND PROFILE**

SCALE	HORIZONTAL	1" = 20'	PROJECT NUMBER: SA 1530	ISSUED FOR: REVIEW	DESIGNED BY: PC	SHEET
	VERTICAL	1" = 2'				
						3 OF 7



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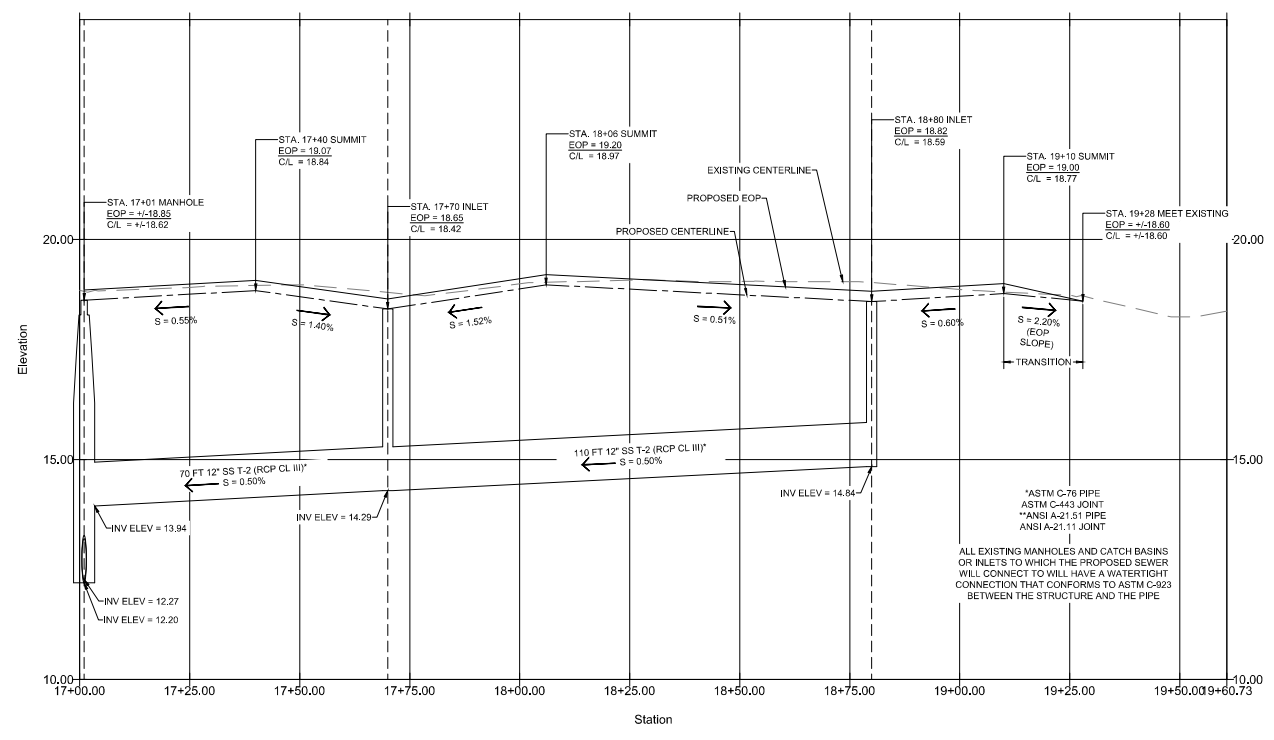
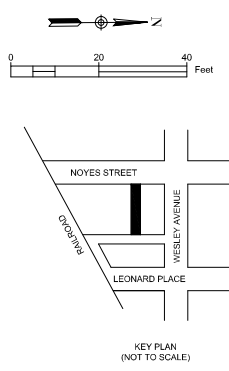
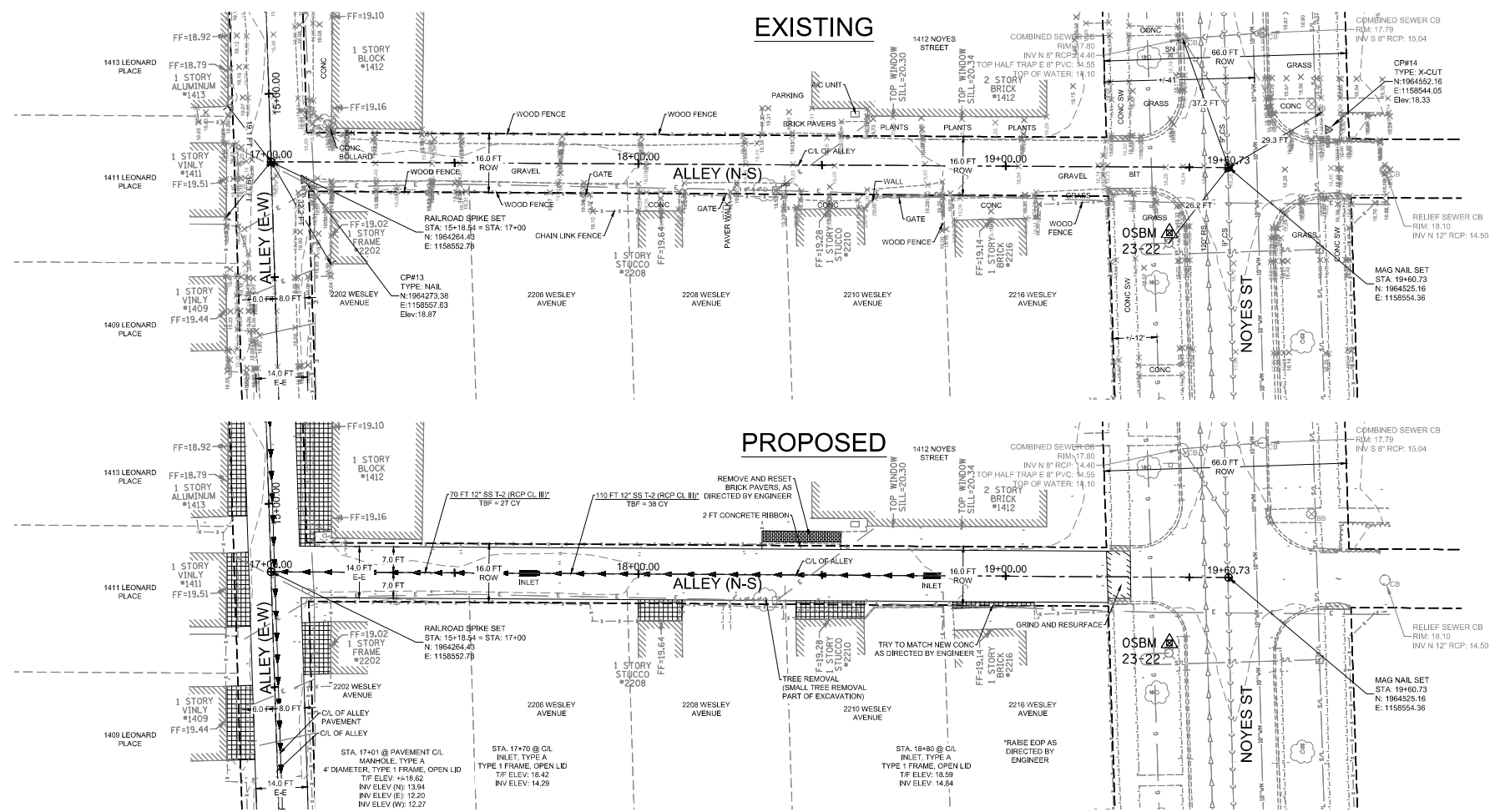


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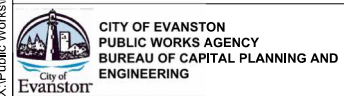
## ALLEY N OF LEONARD PL, E OF GREEN BAY RD PLAN AND PROFILE

SCALE	PROJECT NUMBER: SA 1530	ISSUED FOR: REVIEW	DESIGNED BY: PC	SHEET <b>4 OF 7</b>
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VERTICAL 1" = 2'			CHECKED BY: PC, BN	





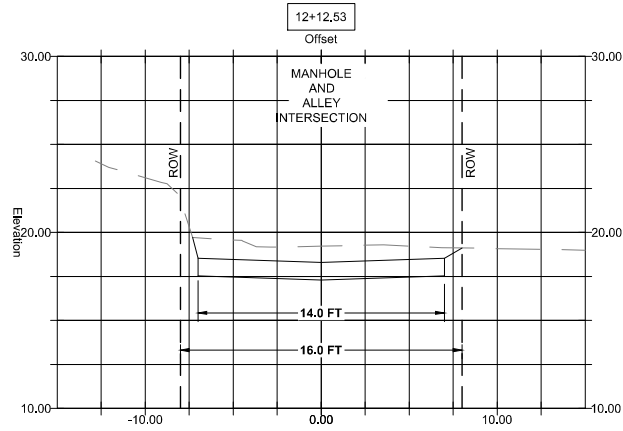
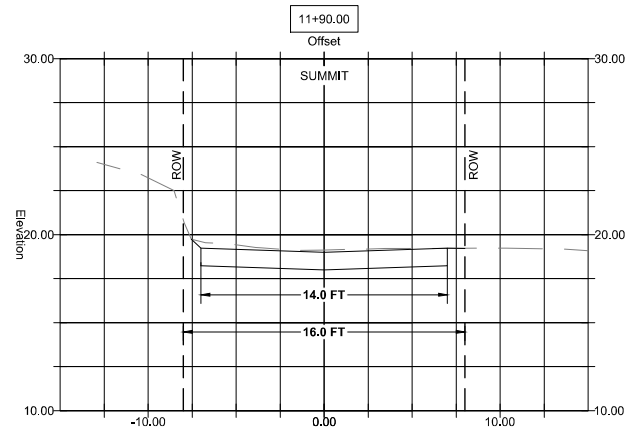
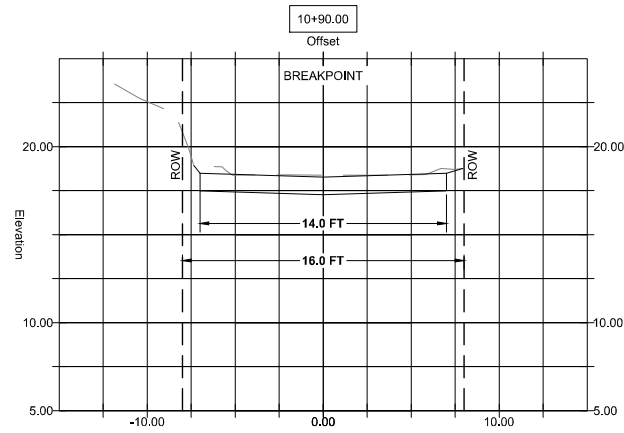
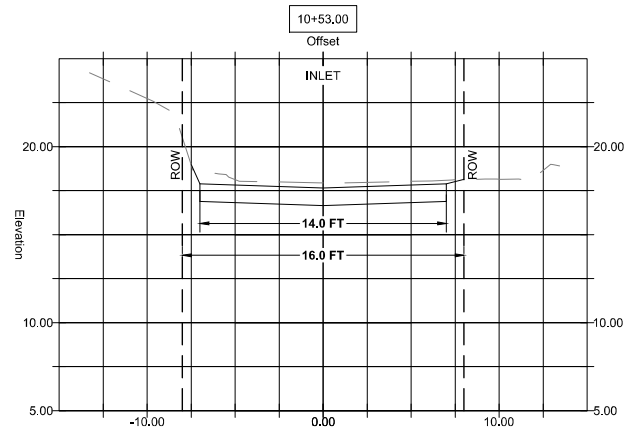
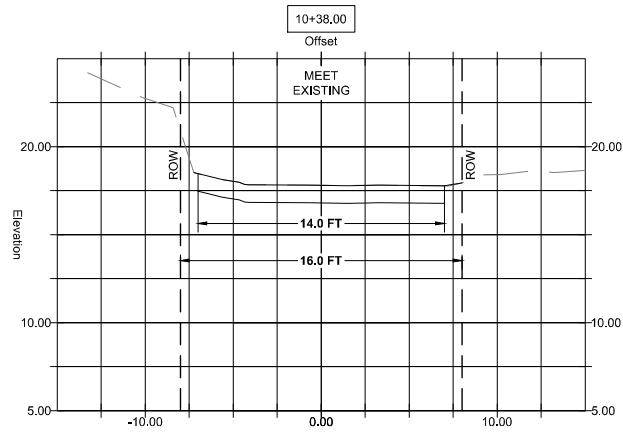
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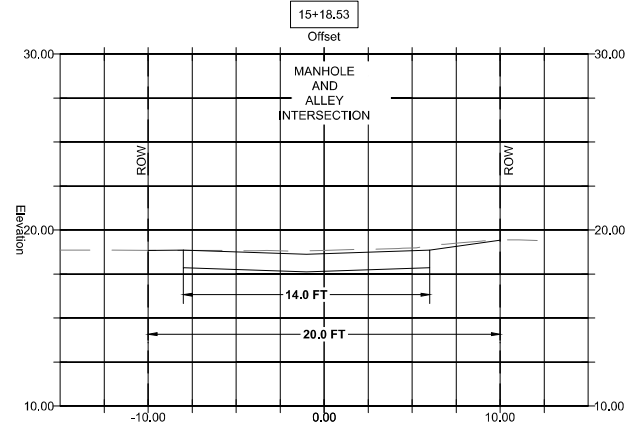
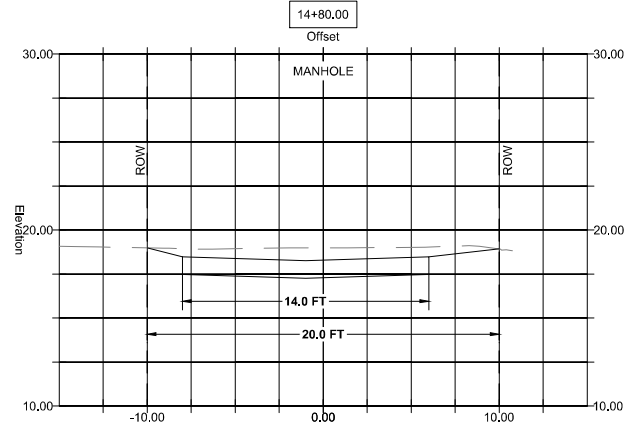
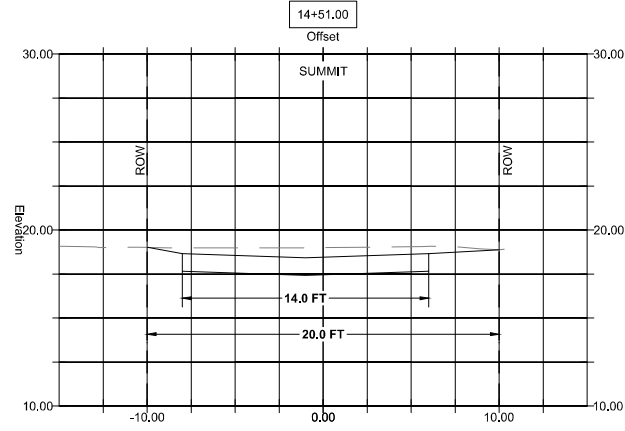
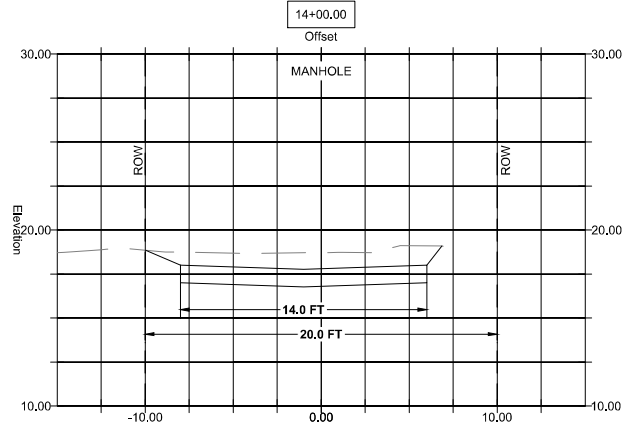
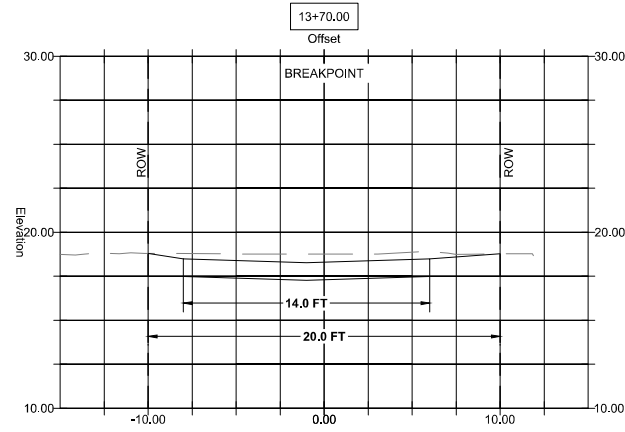
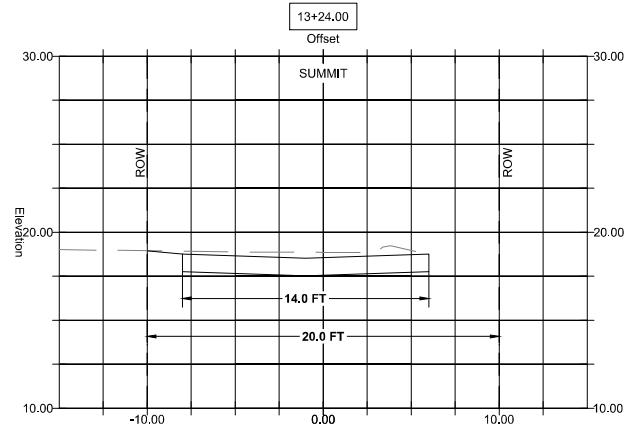
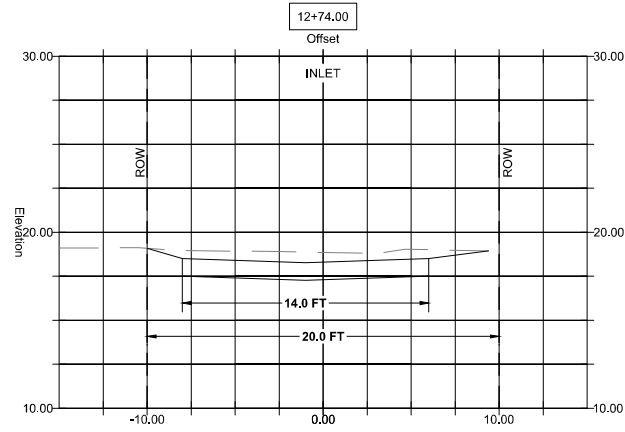
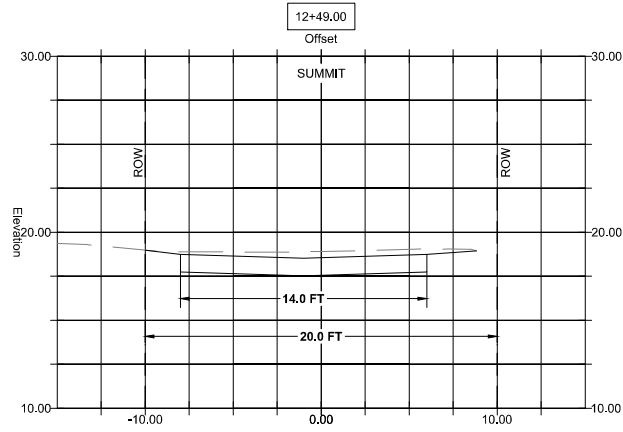
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## ALLEY N OF LEONARD PL, E OF GREEN BAY RD PLAN AND PROFILE

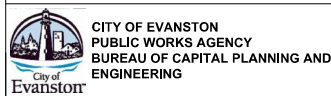
SCALE	HORIZONTAL	1" = 20'	PROJECT NUMBER: SA 1530	ISSUED FOR: REVIEW	DESIGNED BY: PC	SHEET
	VERTICAL	1" = 2'				
						5 OF 7



EARTH EXCAVATION NORTH-SOUTH (WEST) LEG = 107.61 X 1.03 = 110.84 CUBIC YARDS



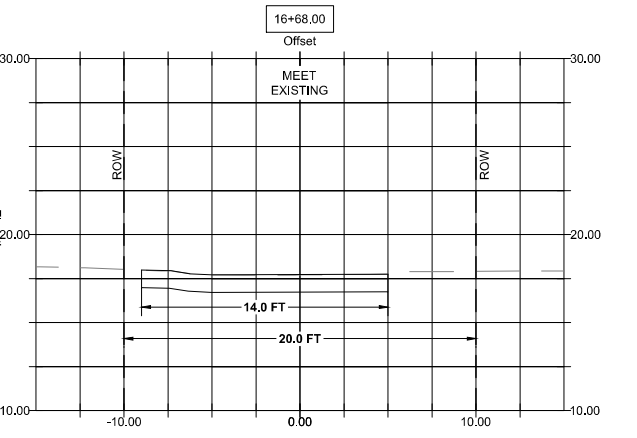
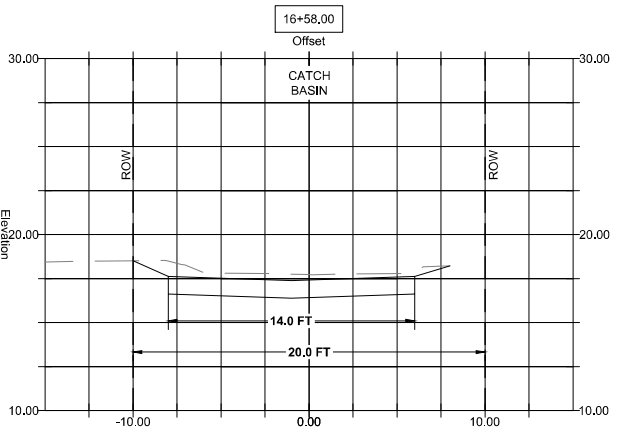
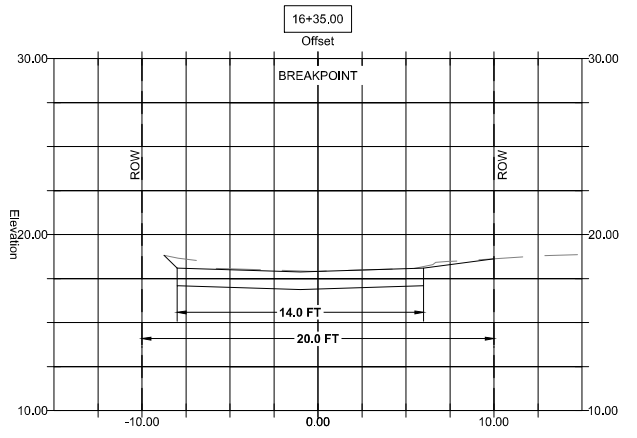
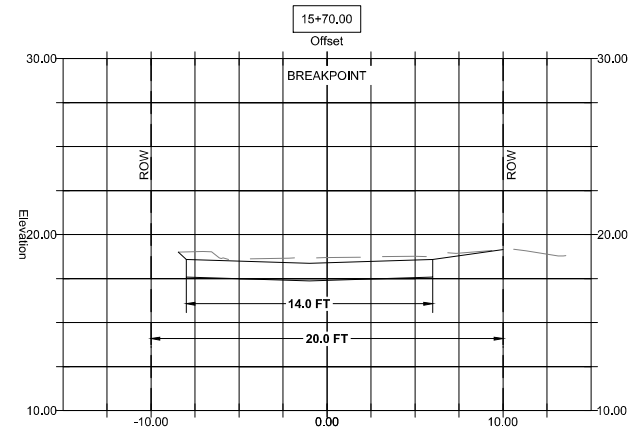
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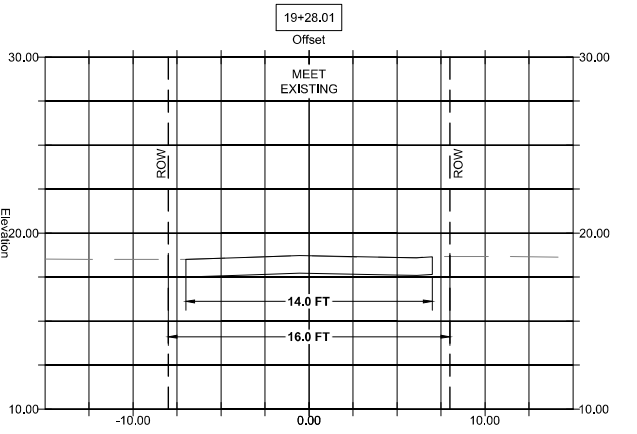
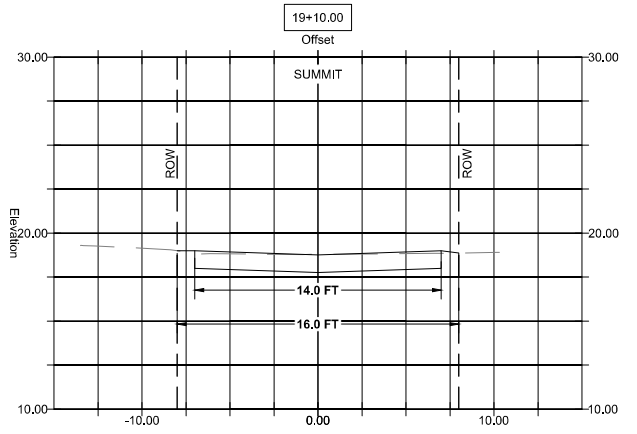
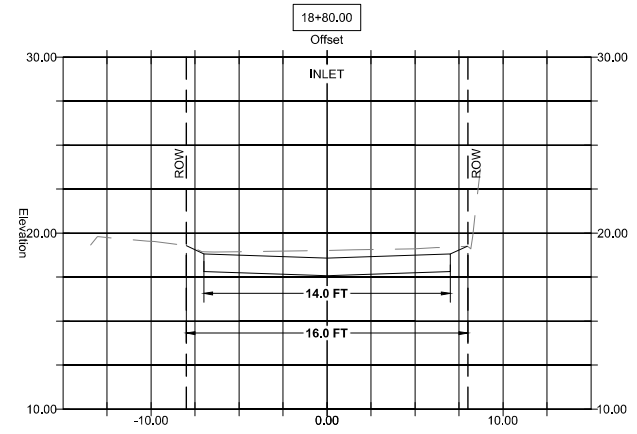
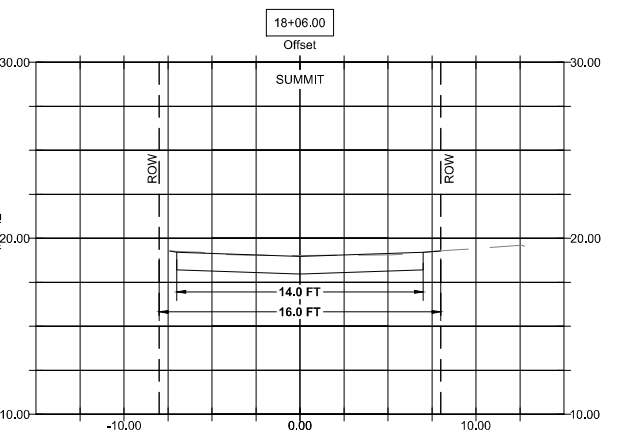
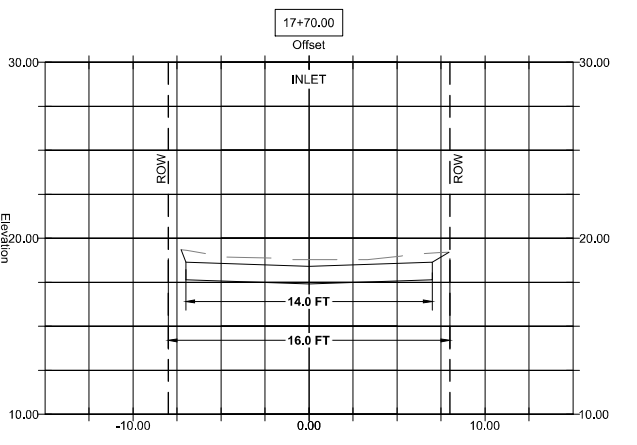
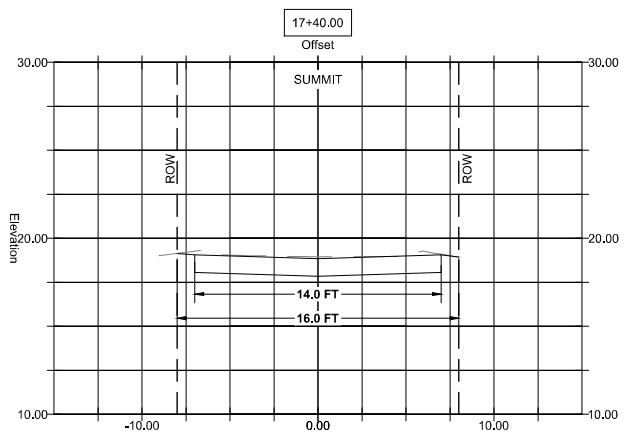
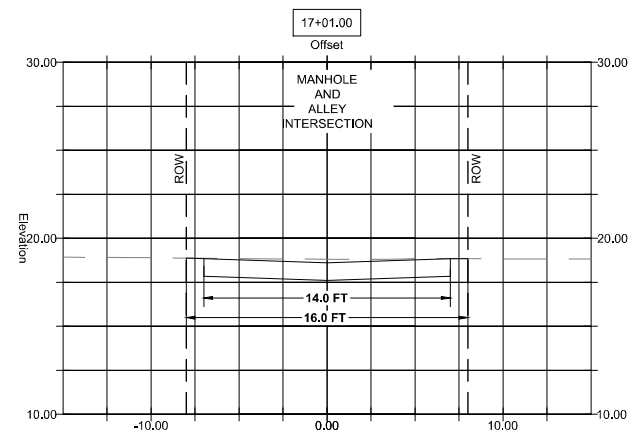
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DATE	NO.	REVISION

## ALLEY N OF LEONARD PL, E OF GREEN BAY RD CROSS SECTIONS

SCALE HORIZONTAL 1" = 5' VERTICAL 1" = 5'	PROJECT NUMBER: SA 1530	ISSUED FOR: REVIEW	DESIGNED BY: PC	SHEET <b>6 OF 7</b>
	BID NUMBER: 24-XX	DATE: 02/14/2024	DRAWN BY: BN	
			CHECKED BY: PC, BN	



EARTH EXCAVATION EAST-WEST LEG =  $322.05 \times 1.03 = 331.71$  CUBIC YARDS



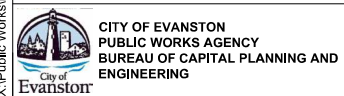
AT ALL UTILITY POLES AND OTHER ENCROACHMENTS, EOP AND CONCRETE CURB ALONG EOP WILL BE FIELD DETERMINED

PREPARATION OF BASE: ALL LABOR, EQUIPMENT, AND MATERIAL NECESSARY FOR BASE PREPARATION SHALL BE INCLUDED IN VARIOUS PAY ITEMS. THIS WORK WILL NOT BE PAID FOR SEPARATELY.

EARTH EXCAVATION NORTH-SOUTH (EAST) LEG =  $135.90 \times 1.03 = 139.98$  CUBIC YARDS

**TOTAL EARTH EXCAVATION = 583 CUBIC YARDS**

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DATE	NO.	REVISION

## ALLEY N OF LEONARD PL, E OF GREEN BAY RD CROSS SECTIONS

SCALE HORIZONTAL 1" = 5' VERTICAL 1" = 5'	PROJECT NUMBER: SA 1530	ISSUED FOR: REVIEW	DESIGNED BY: PC	SHEET <b>7 OF 7</b>
	BID NUMBER: 24-XX	DATE: 02/14/2024	DRAWN BY: BN CHECKED BY: PC, BN	

# ESTIMATE OF ENGINEER OF BOARD OF LOCAL IMPROVEMENTS

To the Board of Local Improvements of the City of Evanston:

Pursuant to a Resolution of the Board of Local Improvements of the City of Evanston, heretofore adopted, providing that the unimproved public alley in the block bounded by Noyes Street on the north, Leonard Place on the south, Green Bay Road on the west, and Wesley Avenue on the east, in the City of Evanston, be improved by excavating, constructing the necessary drainage system and paving with 8" Portland Cement Concrete Pavement, to a width of fourteen feet (14').

Said improvement to be constructed in detail as specified in aforesaid Resolution.

I, herewith submit an estimate of cost of such improvement as described in said Resolution, including labor, materials, and all other lawful expenses attending the proceedings for making said improvement and the cost of making and collecting the assessment therefore as provided by law,

## COST ESTIMATE SPECIAL ASSESSMENT NO: 1530

<u>ITEM NO.</u>	<u>ITEM</u>	<u>UNIT</u>	<u>QUANTITY</u>	<u>UNIT PRICE</u>	<u>TOTAL</u>
1	EARTH EXCAVATION	CU. YD.	583	\$60.00	\$34,980.00
2	TRENCH BACKFILL	CU. YD.	439	\$38.00	\$16,682.00
3	SUB-BASE GRANULAR MATERIAL, TYPE B	CU. YD.	15	\$38.00	\$570.00
4	GRANULAR CRADLE MATERIAL, CA-11	TON	295	\$36.00	\$10,620.00
5	AGGREGATE BASE COURSE, TYPE B,4"	SQ. YD.	1,395	\$8.00	\$11,160.00
6	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	6	\$300.00	\$1,800.00
7	PORTLAND CEMENT CONCRETE PAVEMENT, 8"	SQ. YD.	1,385	\$110.00	\$152,350.00
8	STORM SEWERS, SPECIAL, 10" ( D. I. P. / CL 50 )	FOOT	162	\$145.00	\$23,490.00
9	STORM SEWERS, 12", TYPE 2 ( R. C. P. / CL III )	FOOT	624	\$135.00	\$84,240.00
10	STORM SEWERS, SPECIAL 12" (D.I.P. /CL 50)	FOOT	8	\$190.00	\$1,520.00
11	CATCH BASIN, TYPE A, 4' DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	\$6,000.00	\$6,000.00
12	MANHOLE, TYPE A, 4' DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	4	\$5,400.00	\$21,600.00
13	INLET, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	4	\$3,000.00	\$12,000.00
14	COMBINATION CONCRETE CURB AND GUTTER, TYPE B 6.12	FOOT	90	\$38.00	\$3,420.00
15	CONCRETE CURB , TYPE B	FOOT	175	\$38.00	\$6,650.00
16	PAVEMENT REMOVAL	SQ. YD.	45	\$26.00	\$1,170.00
17	DRIVEWAY PAVEMENT REMOVAL	SQ. YD.	400	\$21.00	\$8,400.00
18	CURB REMOVAL	FOOT.	265	\$7.00	\$1,855.00
19	SIDEWALK REMOVAL	SQ. FT.	310	\$3.00	\$930.00
20	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT - 6"	SQ. YD.	400	\$78.00	\$31,200.00
21	PORTLAND CEMENT CONCRETE SIDEWALK - 5"	SQ. FT.	310	\$11.00	\$3,410.00
22	PROTECTIVE SEALER	SQ. YD.	1,785	\$1.60	\$2,856.00
23	CRUSHED STONE, CA - 14	TON	30	\$35.00	\$1,050.00
24	FURNISHING AND PLACING TOPSOIL, 3"	SQ. YD.	140	\$10.00	\$1,400.00
25	SODDING, SALT TOLERANT	SQ. YD.	140	\$18.00	\$2,520.00
26	DETECTABLE WARNINGS	SQ. FT.	20	\$55.00	\$1,100.00

Estimate of Engineer BLI – S.A. 1530  
(Alley N/Leonard Place, E/Green Bay Road)

<u>ITEM NO.</u>	<u>ITEM</u>	<u>UNIT</u>	<u>QUANTITY</u>	<u>UNIT PRICE</u>	<u>TOTAL</u>
27	TREE REMOVAL (6 TO 15 INCH DIA.)	UNIT. DIA.	11	\$125.00	\$1,375.00
28	HOT-MIX ASPHALT SURFACE REMOVAL	SQ. FT.	35	\$30.00	\$1,050.00
29	TEMPORARY FENCE	FOOT.	140	\$7.00	\$980.00
30	CONSTRUCTION LAYOUT AND STAKING	LUMP SUM	1	\$7,500.00	\$7,500.00
31	STREET SWEEPING	EACH	3	\$400.00	\$1,200.00
32	CLOSED CIRCUIT TV INSPECTION	LUMP SUM	1	\$5,500.00	\$5,500.00
33	PRE-CONSTRUCTION SURFACE VIDEO TAPING	LUMP SUM	1	\$2,000.00	\$2,000.00
34	REMOVING AND RESETTING PAVERS	SQ. FT.	85	\$25.00	\$2,125.00
35	EPOXY PAVEMENT MARKING LINE- 6" YELLOW	FOOT	125	\$40.00	\$5,000.00
				<b>TOTAL</b>	<b>\$469,703.00</b>

Cost of Engineering Services.....	\$21,136.64
For Lawful Expenses, etc.....	\$21,136.64
Sub-Total .....	<b>\$42,273.28</b>

**RECAPITULATION**

Construction Cost.....	\$469,703.00
Engineering.....	\$21,136.64
Legal.....	\$21,136.64
<b>Total Cost.....</b>	<b>\$511,976.28</b>

I hereby certify that in my opinion the above estimate does not exceed the probable cost of the above proposed improvement and the lawful expenses attending the same.

\_\_\_\_\_  
Lara N. Biggs, P.E.  
City Engineer  
Board of Local Improvements

ALLEY NPKTH OF LEONARD PLACE  
EAST OF C. & NW RAIL ROAD.

BOARD OF LOCAL IMPROVEMENTS

ASSESSMENT ROLL

SPECIAL ASSESSMENT NO. 15

UNIT METHOD

ADDRESS (OWNER)	ADDRESS (SITE)	PERMANENT REAL ESTATE INDEX NO	PART OF LOT OR LAND	LEGAL		NO OF UNITS	ASSESSMENT PER UNIT	ASSESSMENT		
				LOT	BLOCK			1st ASSESSMENT	2-10 INSTALLMENT	TOTAL
BLOCK 3 IN MCCORMICK'S SUB DIVISION OF THAT PART OF THE SOUTH EAST 1/4 OF THE SOUTH EAST 1/4 OF SECTION 12, TOWNSHIP 41 NORTH, RANGE 13, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.										
KAREN YORREITER OR CURRENT OWNER	1434 NOYES STREET EV IL 60201	10-12-417-002-0000	PART OF PART OF	17 16	3		2	1629.67	1621.00	16,218.67
ERIC JOHNSON OR CURRENT OWNER	1432 NOYES STREET EV IL 60201	10-12-417-003-0000	PART OF PART OF	16 15	3		1	810.34	811.00	8,109.34
CHARLES DAVIDSON OR CURRENT OWNER	1428 NOYES STREET EV IL 60201	10-12-417-004-0000	PART OF PART OF	15 14	3		1	810.34	811.00	8,109.34
ZITA & GEOFF SWINDELL OR CURRENT OWNER	1424 NOYES STREET EV IL 60201	10-12-417-005-0000	PART OF PART OF	13 12	3		2	1629.67	1621.00	16,218.67
ABE HERNANDEZ OR CURRENT OWNER	1422 NOYES STREET EV IL 60201	231 19TH ST SW 10-12-417-006-0000 NAPLES, FL 34117	PART OF PART OF	11 12	3		2	1629.67	1621.00	16,218.67
OUR GANG PARTNERS OR CURRENT OWNER	1418 NOYES STREET EV IL 60201	343 LANDIS LANE 10-12-417-007-0000 DEERFIELD, IL 60015	PART OF PART OF	10 11	3		2	1629.67	1621.00	16,218.67
SALLY COLLIN OR CURRENT OWNER	1416 NOYES STREET EV IL 60201	10-12-417-008-0000	PART OF PART OF	9 10	3		1	810.34	811.00	8,109.34
MATTHEW PELLEGRITE OR CURRENT OWNER	2216 WESLEY AVE. EV IL 60201	10-12-417-011-0000	ALL OF PART OF	1 2	3		1	810.34	811.00	8,109.34
WILLIAM STEINBAUM OR CURRENT OWNER	2210 WESLEY AVE EV IL 60201	10-12-417-012-0000	PART OF PART OF	2 3	3		1	810.34	811.00	8,109.34
TODD VAN NECK OR CURRENT OWNER	2208 WESLEY AVE. EV IL 60201	10-12-417-013-0000	PART OF ALL OF	3,5 4	3		1	810.34	811.00	8,109.34
NANCY BURKE OR CURRENT OWNER	2206 WESLEY AVE EV IL 60201	10-12-417-014-0000	PART OF PART OF	5 6	3		1	810.34	811.00	8,109.34

SPECIAL ASSESSMENT NO. 15

ADDRESS (OWNER)	ADDRESS (SITE)	PERMANENT REAL ESTATE INDEX NO	PART OF LOT OR LAND	LEGAL		NO OF UNITS	ASSESSMENT PER UNIT	ASSESSMENT		
				LOT	BLOCK			1st ASSESSMENT	2-10 INSTALLMENT	TOTAL
MICHAEL KENNEDY OR CURRENT OWNER	2202 WESLEY AVE EV. IL. 60201	10-12-417-015-0000	PART OF ALL OF	6 7	3		1	810.34	811.00	8,109.34
PATRICK HECHINGER OR CURRENT OWNER	1412 NOYES STREET EV. IL. 60201	10-12-417-027-0000	ALL OF PART OF	8 9	3		1	810.34	811.00	8,109.34
BLOCK 3, IN EVANSTON CENTRE SECOND ADDITION BEING THE EAST 395 FEET OF THE SOUTH 708-1/2 FEET OF THE SOUTH EAST 1/4 OF SECTION 12 TOWNSHIP 41 NORTH, RANGE 13 EAST OF THE THIRD PRINCIPAL MERIDIAN IN COOK COUNTY, ILLINOIS.										
INDRE RACKAUSKAS OR CURRENT OWNER	1427 LEONARD PLACE EV. IL. 60201	10-12-417-016-0000	ALL OF	9	3		2	1629.67	1621.00	16,218.67
JULIAN SOTNICK OR CURRENT OWNER	1423 LEONARD PLACE EV. IL. 60201	10-12-417-017-0000	ALL OF	8	3		1	810.34	811.00	8,109.34
SHAUN JANSALL OR CURRENT OWNER	1421 LEONARD PLACE EV. IL. 60201	10-12-417-018-0000	ALL OF	7	3		1	810.34	811.00	8,109.34
MATTHEW DUSTAL OR CURRENT OWNER	1415 LEONARD PLACE EV. IL. 60201	10-12-417-019-0000	ALL OF	6	3		1	810.34	811.00	8,109.34
LIANE SEBASTIAN OR CURRENT OWNER	1413 LEONARD PLACE EV. IL. 60201	10-12-417-020-0000	ALL OF	5	3		2	1629.67	1621.00	16,218.67
MICHAEL HONIBALL OR CURRENT OWNER	1411 LEONARD PLACE EV. IL. 60201	10-12-417-021-0000	ALL OF	4	3		1	810.34	811.00	8,109.34
JOSEPH P. LARMEE OR CURRENT OWNER	1409 LEONARD PLACE EV. IL. 60201	10-12-417-022-0000	ALL OF	3	3		1	810.34	811.00	8,109.34

TOTAL:

COST PER UNIT: \$ / =





STATE OF ILLINOIS  
COOK COUNTY  
CITY OF EVANSTON  
PLANS FOR  
**PAVING OF ALLEY NORTH OF COLFAX STREET,  
EAST OF BRYANT AVENUE**  
SA 1531  
BID #24-XX

INDEX OF SHEETS

- 1 COVER SHEET
- 2 GENERAL NOTES, SUMMARY OF QUANTITIES, AND TYPICAL SECTIONS
- 3-5 ALLEY NORTH OF COLFAX STREET, EAST OF BRYANT AVENUE PLAN AND PROFILE
- 6-7 ALLEY NORTH OF COLFAX, EAST OF BRYANT AVENUE CROSS SECTIONS

GENERAL NOTES

- ANY REFERENCE TO "STANDARD" THROUGHOUT THE PLANS, SPECIFICATIONS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO THE LATEST STANDARDS OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.
- SAWING OF EXISTING SURFACES, WHEN REQUIRED FOR REMOVAL OR CONSTRUCTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE VARIOUS PAY ITEMS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING THE CONSTRUCTION SHALL BE REPAIRED OR REPLACED BY THE THE CONTRACTOR AT HIS OWN EXPENSE.
- THE CONTRACTOR WILL BE REQUIRED TO CONSTRUCT ALL NEW FRAMES AND LIDS IN CONFORMANCE WITH DETAILS OF STANDARD FRAMES AND LIDS SHOWN IN THE PLANS.
- PROTECTIVE COAT WILL BE APPLIED TO CONCRETE CURB AND SIDEWALK. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

NOTE:

- EXISTING CONDITIONS ARE BASED UPON A TOPOGRAPHIC SURVEY. THIS IS NOT A BOUNDARY SURVEY. PROPERTY LINES SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY AND SHALL NOT BE USED AS A BASIS FOR CONSTRUCTION LAYOUT.
- CONTRACTOR WILL BE REQUIRED TO ESTABLISH ACTUAL PROPERTY LINES AND CENTER LINE OF ROW FOR PROPOSED IMPROVEMENT, AS DIRECTED BY ENGINEER.
- THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR "CONSTRUCTION AND LAYOUT" PAY ITEM.

**NOTE:**  
THE PROPOSED PROJECT WILL NOT ALTER OR HAVE IMPACT ON THE FLOOD PROTECTION AREAS AND THE PROPOSED DRAINAGE CONDITION OF THE PROJECT SITES.

SIGNED BY: BRIDGET NASH, PE  
SENIOR PROJECT MANAGER



PROJECT LENGTH: 554 FEET (0.105 MILES)

**NOTICE**  
NO WORK TO BE DONE  
WITHIN THE PAVED  
ALLEY AREA FOR FIVE  
YEARS BY ANY UTILITY

FOR UNDERGROUND UTILITY INFORMATION  
AND LOCATIONS, CALL  
**J.U.L.I.E**  
TOLL FREE  
TELEPHONE No. 1-800-892-0123

STANDARD SYMBOLS

→	EXISTING GUY WIRE	→	PROPOSED GUY WIRE	⬮	EX DETECTABLE WARNING
⊠	EXISTING POWERPOLE	⊠	PROPOSED POWERPOLE	⬮	PROP DETECTABLE WARNING
⊗	EXISTING STREET LIGHT	⊗	PROPOSED STREET LIGHT	⬮	DRIVEWAY REMOVAL AND REPLACEMENT (CONCRETE)
⊗	EXISTING BUSH	⊗	PROPOSED BUSH	⬮	DRIVEWAY REMOVAL AND REPLACEMENT (ASPHALT)
⊗	EXISTING STUMP	●	PROPOSED CATCH BASIN	⬮	PAVEMENT REMOVAL AND REPLACEMENT
⊗	EXISTING TREE	●	PROPOSED MANHOLE	⬮	PAVEMENT REMOVAL
○	EXISTING CATCH BASIN	—	PROPOSED CENTERLINE	⬮	HMA SURFACE REMOVAL
○	EXISTING INLET	→ 12" CS	PROPOSED COMBINED SEWER		
○	EXISTING MANHOLE	→ 12" SAN	PROPOSED SANITARY SEWER		
△	EXISTING CONTROL POINT	—	EXISTING SIDEWALK		
—	EXISTING CENTERLINE	→ 12" SS	PROPOSED STORM SEWER		
→ 12" CS	EXISTING COMBINED SEWER	→ 8" W	PROPOSED WATER (SIZE VARIES)		
—	EXISTING ROW				
→ 12" SAN	EXISTING SANITARY SEWER				
—	EXISTING SIDEWALK				
→ 12" SS	EXISTING STORM SEWER				
→ 8" W	EXISTING WATER MAIN				
—	EXISTING FENCE				
⊠	EXISTING SIGN	⊠	PROPOSED SIGN		
↔	EXISTING SUMMIT	⊕	PROPOSED SUMMIT		
		⊕	CB / MH / VV TO BE FILLED		
		⊗	ELEMENT TO BE REMOVED		
		A ⊗	CB / MH / VV TO ADJUSTED		
		R ⊗	CB / MH / VV TO RECONSTRUCTED		
		⊗	PROPOSED VALVE BOX		
		A ⊗	VALVE BOX TO BE ADJUSTED		
—	EXISTING CURB TYPE B	—	PROPOSED CURB TYPE B		
—	EXISTING CURB AND GUTTER TYPE B6-12	—	PROPOSED CURB AND GUTTER TYPE B6-12		

NOTE:

- SEE THE CONTRACT SPECIFICATIONS FOR TYPICAL GENERAL NOTES PERTAINING TO THE REQUIREMENTS AND CONDITIONS OF THE METROPOLITAN WATER RECLAMATION DISTRICT OF GREATER CHICAGO.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL PRIVATE PROPERTIES ABUTTING THE WORK AREA OF THE PROJECT. ANY PRIVATE PROPERTY THAT IS DAMAGED DURING THE CONSTRUCTION PERIOD SHALL BE REPAIRED OR REPLACED TO ITS ORIGINAL CONDITION AT THE CONTRACTORS' EXPENSE. IN ORDER TO DOCUMENT THE ORIGINAL CONDITION OF ALL PRIVATE PROPERTIES ABUTTING THE WORK AREA, PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE PROPERTY OWNERS AND MAKE A VIDEOTAPE RECORD OF THE PROJECT AREA. THIS SHALL INCLUDE THE CONDITION AND IDENTIFICATION (BY ADDRESS) OF ALL FENCES, GARAGES AND GARAGE FLOORS. THIS RECORD SHALL BE USED TO RESOLVE ANY DAMAGE CLAIMS THAT MAY ARISE AFTER THE CONSTRUCTION. A COPY OF THE VIDEOTAPE SHALL BE FURNISHED TO THE RESIDENT ENGINEER PRIOR TO THE START OF CONSTRUCTION.

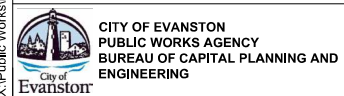
FLEXIBLE RUBBER CONNECTOR REQUIREMENT

"ALL INLET AND OUTLET PIPES OF SANITARY SEWER MANHOLES AND OTHER UNDERGROUND STRUCTURES, ALL COMBINED AND/OR STORM SEWER MANHOLES, CATCHBASINS, INLETS, AND UNDERGROUND DETENTION STORAGE STRUCTURES, SHALL BE JOINED WITH WATERTIGHT FLEXIBLE RUBBER CONNECTORS CONFORMING TO A.S.T.M. C-443 AND C-923 WITH STAINLESS STEEL BAND."

CITY OF EVANSTON SEWER SYSTEM OWNERSHIP

"ALL COMBINED SEWER AND/OR STORM SEWER SEWER SYSTEM INCLUDING ALL INTERMEDIATE SEWER SYSTEM IS OWNED BY THE CITY OF EVANSTON."

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**ALLEY N OF COLFAX ST, E OF BRYANT AVE  
COVER SHEET**

SCALE	PROJECT NUMBER: SA 1531	ISSUED FOR: REVIEW	DESIGNED BY: PC	SHEET <b>1 OF 7</b>
HORIZONTAL N/A	BID NUMBER: 24-XX	DATE: 02/14/2024	DRAWN BY: BN	
VERTICAL N/A			CHECKED BY: PC, BN	

GENERAL NOTES

THE ILLINOIS DEPARTMENT OF TRANSPORTATION 'STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION' LATEST EDITION, THE 'STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS' LATEST EDITION, PROJECT SPECIFICATIONS, ALL APPLICABLE REQUIREMENTS OF THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, THE RESPECTIVE CITY, ALL APPLICABLE REQUIREMENTS OF THE ORDINANCES OF AUTHORITIES HAVING JURISDICTION AND ALL ADDENDA THERETO SHALL GOVERN THIS WORK.

THE STANDARD SPECIFICATIONS, PROJECT SPECIFICATIONS, CONSTRUCTION PLANS AND SUBSEQUENT DETAILS ARE ALL TO BE CONSIDERED AS PART OF THE CONTRACT. INCIDENTAL ITEMS OR ACCESSORIES NECESSARY TO COMPLETE THIS WORK MAY NOT BE SPECIFICALLY NOTED BUT ARE TO BE CONSIDERED A PART OF THE CONTRACT.

THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IN ADDITION, THE CONTRACTOR MUST VERIFY THE ENGINEER'S LINE AND GRADES. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTION FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSION OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTION, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.

BEFORE ACCEPTANCE BY THE OWNER AND FINAL PAYMENT, ALL WORK SHALL BE INSPECTED AND APPROVED BY THE OWNER OR HIS REPRESENTATIVES. FINAL PAYMENT WILL BE MADE AFTER ALL OF THE CONTRACTOR'S WORK HAS BEEN APPROVED AND ACCEPTED. WHENEVER DURING CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF GUTTERS, DRAINAGE STRUCTURES, DITCHES, ETC., SUCH THAT THE NATURAL FLOW LINE OF WATER IS OBSTRUCTED, THE LOOSE MATERIAL WILL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES AND FLOW LINES SHALL BE FREE FROM DIRT AND DEBRIS. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR'S FAILURE TO PROVIDE THE ABOVE WILL PRECLUDE ANY POSSIBLE ADDED COMPENSATION REQUESTED DUE TO DELAYS OR UNSUITABLE MATERIALS CREATED AS A RESULT THEREOF.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SIGNS, TRAFFIC CONTROL DEVICES, AND WARNING DEVICES TO INFORM AND PROTECT THE PUBLIC DURING ALL PHASES OF CONSTRUCTION.

WHENEVER THE PERFORMANCE OF WORK IS INDICATED ON THE PLANS AND NO ITEM IS INCLUDED IN THE CONTRACT FOR PAYMENT, THE WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

DURING CONSTRUCTION OPERATIONS THE CONTRACTOR SHALL ENSURE POSITIVE SITE DRAINAGE OF EACH DAY. SITE DRAINAGE MAY BE ACHIEVED BY DITCHING, CHANNELING, PUMPING OR ANY OTHER METHOD ACCEPTABLE TO THE ENGINEER AND RESPECTIVE CITY. THE CONTRACTOR'S FAILURE TO PROVIDE THE ABOVE WILL PRECLUDE ANY POSSIBLE ADDED COMPENSATION REQUESTED DUE TO DELAYS OR UNSUITABLE MATERIALS CREATED AS A RESULT THEREOF.

ALL CONSTRUCTION WILL BE INSPECTED BY THE ENGINEER AND THE CITY, SPECIFICALLY ALL TRENCHES AND SEWERS SHALL BE LEFT OPEN (BUT SAFELY BARRICADED) UNTIL INSPECTED AND APPROVED BY THE CITY ENGINEER. PAVEMENT SUBBASE, BASE, AND SURFACE MUST EACH BE INSPECTED BY THE ENGINEER AND THE RESPECTIVE CITY PRIOR TO THE NEXT PHASE OF WORK. PROOF ROLLING AND NUCLEAR DENSITY TESTING WILL BE UTILIZED.

IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REMOVE FROM THE SITE ANY AND ALL MATERIALS AND DEBRIS WHICH RESULT FROM HIS CONSTRUCTION OPERATIONS AT NO ADDITIONAL EXPENSE TO THE OWNER.

WHEN A CONFLICT BETWEEN PLANS AND SPECIFICATIONS OR NOTES OCCURS, THE ENGINEER SHALL DECIDE WHICH GOVERNS. GENERALLY, THE MORE RESTRICTIVE, MORE SPECIFIC, OR STRICTER PROVISION SHALL GOVERN.

CONTRACTOR IS RESPONSIBLE FOR RETURNING ALL AREAS AFFECTED BY EQUIPMENT OR LABORERS TO EXISTING CONDITIONS. CONTRACTOR IS ALSO RESPONSIBLE FOR PROTECTING ALL NEW WORK UNTIL COMPLETION OF THIS CONTRACT.

THE CONTRACTOR SHALL INDEMNIFY THE OWNER, THE ENGINEER, THE RESPECTIVE CITY AND THEIR AGENTS, FROM ALL LIABILITY INVOLVED IN THE CONSTRUCTION AND INSTALLATION AND TESTING OF THE WORK ON THIS PROJECT. THE CONTRACTOR MUST CARRY INSURANCE IN ACCORDANCE WITH CITY SPECIFICATIONS AND MUST PRESENT A JOB-SPECIFIC CERTIFICATE OF INSURANCE NAMED ALL EMPLOYEES OF THE OWNER, THE CITY, AND THE ENGINEER, AS ADDITIONAL INSURED.

EXISTING UTILITIES: WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF THE ENGINEER AS TO THE LOCATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE CONTRACTOR. THE ENGINEER AND OWNER ASSUME NO RESPONSIBILITY WHATSOEVER IN RESPECT TO THE SUFFICIENCY OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS RELATIVE TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES OR THE MANNER IN WHICH THEY ARE TO BE REMOVED OR ADJUSTED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES. HE SHALL ALSO OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES, JULIE, THE RESPECTIVE CITY, DETAILED INFORMATION RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULES OF THE UTILITY COMPANIES FOR REMOVING OR ADJUSTING THEM.

ALL WORK PERFORMED UNDER THIS CONTRACT SHALL BE GUARANTEED BY THE CONTRACTOR AND HIS SURETY FOR A PERIOD OF 12 MONTHS FROM THE DATE OF INITIAL ACCEPTANCE OF THE WORK BY THE OWNER AGAINST ALL DEFECTS IN MATERIALS AND WORKMANSHIP OF WHATEVER NATURE.

REMOVED PAVEMENT, SIDEWALK, CURB AND GUTTER, ETC. SHALL BE DISPOSED OFF SITE AT LOCATIONS PROVIDED BY THE CONTRACTOR AT HIS EXPENSE.

CONSTRUCTION MEANS, METHODS, AND JOBSITE SAFETY ARE THE SOLE AND EXCLUSIVE RESPONSIBILITY OF THE CONTRACTOR.

CONSTRUCTION ACTIVITIES MAY OCCUR BETWEEN 7:00 AM AND 5:00 PM MONDAY THROUGH FRIDAY AND 8:00 AM TO 4:00 PM ON SATURDAY (AS APPROVED BY THE ENGINEER). CONSTRUCTION ACTIVITIES ON SUNDAY ARE PROHIBITED. NO WORK WILL BE PERFORMED ON STATE OF ILLINOIS OBSERVED HOLIDAYS. ANY VIOLATION FOR WORKING HOURS CONTRACTOR WILL BE FINED \$500.00 FOR OCCURRENCE.

PAVING AND GRADING NOTES

ALL PAVEMENT DIMENSIONS ARE TO THE EDGE OF PAVEMENT OR FACE OF CURB, UNLESS OTHERWISE INDICATED. CURB RADI ARE TO THE BACK OF CURB UNLESS OTHERWISE INDICATED.

PAVING WORK SHALL INCLUDE FINAL SUB-GRADE SHAPING AND PREPARATION, FORMING, PLACEMENT OF BASE COURSE MATERIALS, AND SUBSEQUENT BINDER AND/OR SURFACE COURSES, FINISHING AND CURING OF CONCRETE, FINAL CLEAN-UP AND ALL RELATED WORK.

THE PROPOSED PAVEMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.

PAVEMENT DESIGN SHALL BE AS DETAILED, PRIOR TO THE PLACEMENT OF ANY STONE BASE ALL SUBBASE SHALL BE COMPACTED WITH A SELF-PROPELLED SHEEPSFOOT COMPACTOR (CAT #15 OR LARGER) TO A MINIMUM DENSITY OF 95% MOISTURE PROCTOR. TESTING SHALL BE BY NUCLEAR DENSITY TEST AND PROOF ROLLING. THE CONTRACTOR SHALL PROOF ROLL THE SUBGRADE BEFORE PAVEMENT CONSTRUCTION PROCEEDS. THE PROOF ROLL MUST BE WITNESSED BY THE ENGINEER OR HIS REPRESENTATIVE. THE SUBGRADE WILL NOT BE APPROVED AND ACCEPTED WITHOUT PROOF ROLLING. THE PROOF ROLLING SHALL BE DONE BY A FULLY LOADED, THREE-AXLE DUMP TRUCK TOGETHER WITH LOAD WEIGHING AT LEAST TWENTY-FIVE (25) TONS OR BASE HAS FAILURE OR PUMPING AS INDICATED. BY PROOF ROLLING, THE AREA OF FAILURE OR PUMPING SHALL BE SCARIFIED AND RECOMPACTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. SUBSEQUENT PROOF ROLL WILL BE CONDUCTED UNTIL THE SUBGRADE IS FOUND TO BE ACCEPTABLE TO THE ENGINEER.

A MODIFIED PROCTOR DENSITY OF 95% IS REQUIRED FOR STONE BASE. A DENSITY OF 93% IS REQUIRED FOR ASPHALT. ALL OFF-ROAD ZONES SHALL BE COMPACTED TO A MINIMUM OF 90% STANDARD PROCTOR. FILL CANNOT INCLUDE DEBRIS. (REMOVE ALL DEBRIS, TREES, ETC. FROM SITE).

ALL DISTURBED NON-PAVEMENT AREAS SHALL BE ROUGH GRADED. THE CONTRACTOR IS RESPONSIBLE FOR ALL EROSION PREVENTION AND REPAIR.

CURB AND GUTTER SHALL BE 6x12, VERTICAL CURB OR MATCH EXISTING FOR SHORT SECTIONS. THE CURB SHALL CONSIST OF PORTLAND CEMENT CONCRETE, 4" SLUMP, 6 1/2 BAG MIX, AND AIR ENTRAINMENT OF NOT LESS THAN FIVE (5%) OR MORE THAN EIGHT (8%). CONCRETE SHALL BE MINIMUM COMPRESSIVE STRENGTH (3000 PSI) AT FOURTEEN (14) DAYS. ALL CURB AND GUTTER SHALL BE BROOM FINISHED. SAW CONTRACTION JOINTS AT LEAST 2" IN DEPTH 15' O.C. AND AT ALL 10'S & 15'S WITHIN 24 HOURS. CURB AND GUTTER SHALL BE BACKFILLED AFTER PAVEMENT IS INSTALLED. IT SHALL BE BACKFILLED AND COMPACTED FOR A DISTANCE OF 5 FEET AT A 2% GRADE.

3/4" THICK PRE-MOLDED FIBER EXPANSION JOINTS WITH 3/4" x 18" PLAIN ROUND STEEL DOVEL BARS SHALL BE INSTALLED IN ALL CURBS, 1/2" x 12" PLAIN ROUND STEEL DOVEL BARS IN SIDEWALK AT 10'S SIXTY (60) FEET INTERVALS AND AT ALL T.C.'S, P.T.'S. CURB RETURNING AFTER PAVEMENT IS INSTALLED. THE DOVEL BARS SHALL BE GREASED AND FITTED WITH METAL EXPANSION TUBES. ALL EXPANSION JOINTS MUST BE FREE OF CONCRETE FOR FULL DEPTH.

CONTRACTION JOINTS SHALL BE TOOLED AT 5' INTERVALS IN THE SIDEWALK. THE COST OF THESE JOINTS SHALL BE CONSIDERED AS INCIDENTAL TO THE COST OF THE CONTRACT. CONTRACTION JOINTS SHALL ALSO BE TOOLED DOWN THE CENTER OF ALL SIDEWALKS GREATER THAN 6' WIDE. (FIVE FOOT SPACING MAXIMUM).

ALL PORTLAND CEMENT CONCRETE SHALL BE CURED AND PROTECTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.

CONCRETE SURFACE (SIDEWALKS & DRIVEWAY PAVEMENTS)

- A) FINISH WITH A LIGHT BROOM FINISH.
B) 6 1/2 BAG MIX, 4" SLUMP MAX. 5%±-2% AIR-ENTRAINED, READY-MIX CONCRETE.
C) 3500 PSI AT 14 DAYS, TWO CYLINDERS/60 CY.
D) SLOPE AT MINIMUM 1%, OR AS NOTED ON PLANS.

WHENEVER NEW CONCRETE ABUTS EXISTING CONCRETE, SET A 3/4" THICK PRE-MOLDED FIBER EXPANSION JOINT AND 3/4" IDOT STANDARD EXPANSION ANCHOR TIES IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. THIS INCLUDES CONCRETE POURED ADJACENT TO EXISTING SIDEWALKS, CURBS AND BUILDINGS. THE DOVEL BARS SHOULD BE 4" INTO EXISTING CONCRETE WITH 8" EXTENDING INTO NEW CONCRETE.

PRIME COAT FOR THE SURFACE COURSE SHALL BE APPLIED TO THE BINDER AT A RATE OF 0.07 GAL/SY (SS-1). PRIME COAT FOR THE BINDER COURSE SHALL BE APPLIED TO THE SUBBASE AT A RATE OF 0.25 GAL/SY (P.E.P.). PRIME COAT WILL NOT BE PAID FOR SEPARATELY.

WHEREVER A PAVEMENT OVERLAY OCCURS ADJACENT TO A CONCRETE SURFACE OR EXISTING PAVEMENT, THE CONTRACTOR SHALL GRIND DOWN (OR REMOVE) ASPHALT 3" WIDE BY 1.5" DEPTH UNLESS OTHERWISE NOTED ON THE PLANS. THIS IS CONSIDERED INCIDENTAL TO THE CONTRACT.

ALL CURB AND SIDEWALK SHALL BE REINFORCED WITH TWO #4 REBARS (THREE EQUALLY SPACED REBAR FOR SIDEWALK) WHENEVER THE CURB OR SIDEWALK CROSSES A UTILITY TRENCH. EXTEND THE REBAR TEN FEET BEYOND THE TRENCH ON BOTH SIDES.

ALL PAVEMENT MARKING SHALL BE EPOXY PAVEMENT MARKINGS.

ALL SIGN POSTS SHALL CONFORM TO IDOT AND MUTCD STANDARDS.

PORTLAND CEMENT CONCRETE PAVEMENT

GENERAL

PORTLAND CEMENT CONCRETE PAVEMENT SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 420 OF THE 'STANDARD SPECIFICATIONS', EXCEPT THAT THE FINAL FINISH SHALL BE OBTAINED BY THE USE OF TWO (2) SEPARATE BURLAP DRAGS HAVING A DOVEL BAR OR MORE AT LEAST FOUR (4) FEET WIDE AND TWO (2) FEET LONGER THAN WIDTH OF SLAB UNDER CONSTRUCTION. THE BURLAP SHALL BE ATTACHED TO A BRIDGE RIDING ON PAVEMENT FORMS AND BE KEPT SATURATED WHILE IN USE. IT SHALL BE LAID ON THE PAVEMENT SURFACE AND DRAGGED IN THE DIRECTION THE PAVEMENT IS BEING LAID, WITH APPROXIMATELY TWO (2) FEET OF WIDTH IN CONTACT WITH THE PAVEMENT SURFACE. THE BURLAP DRAG SHALL BE KEPT CLEANED AND FREE OF HARDENED CONCRETE.

THE ENGINEER MAY REQUIRE CHANGES IN BELTING, BURLAPPING AND EQUIPMENT TO PRODUCE DESIRED FINAL SURFACE TEXTURE.

PREPARATION BASE

ALL LABOR, EQUIPMENT AND MATERIAL NECESSARY FOR BASE PREPARATION SHALL BE INCLUDED IN THE VARIOUS PAY ITEMS. THIS WORK WILL NOT BE PAID FOR SEPARATELY.

TRANSVERSE EXPANSION JOINTS

TRANSVERSE EXPANSION JOINTS SHALL BE PLACED THROUGHOUT THE ENTIRE IMPROVEMENT ACROSS THE CONCRETE PAVEMENT AT RIGHT ANGLES TO THE EDGE THEREOF AND AT INTERVALS OF SIXTY FEET (60') MEASURED ALONG SAID CENTER LINE. THE EXPANSION JOINTS SHALL BE MADE BY PLACING AND LEAVING IN PLACE PRIOR TO LAYING THE CONCRETE MIXTURE FOR THE PAVEMENT, BITUMINOUS PRE-MOLDED JOINT FILLER CONFORMING TO THE REQUIREMENTS OF ARTICLE 751.03 OF THE 'STANDARD SPECIFICATIONS'. SAID JOINT FILLER SHALL BE ONE INCH (1") IN THICKNESS AND SHALL EXTEND VERTICALLY FROM A POINT ONE INCH (1") BELOW THE TOP OF THE SUBGRADE TO A POINT ONE-HALF INCH (1/2") BELOW THE FINISHED GRADE OF THE PAVEMENT. THE INTERVENING SPACE BETWEEN THE TOP OF SAID JOINT FILLER AND THE FINISHED GRADE OF THE PAVEMENT SHALL BE FILLED WITH ASPHALT P.A.F.-3.

LOAD TRANSFER ACROSS SAID EXPANSION JOINT SHALL BE ACCOMPLISHED BY INSTALLATION OF ONE INCH (1") ROUND, PRE-COATED, SMOOTH STEEL BARS EIGHTEEN INCHES (18") LONG AT TWELVE INCH (12") CENTER THROUGH SAID JOINT FILLER. OR BY CASTING AND PLACING AND APPROVED BY THE ENGINEER. ONE END OF EACH OF SAID STEEL BAR SHALL BE FURNISHED WITH A METAL OR FIBER CAP TO PROVIDE ONE INCH (1") OF EXPANSION SPACE. SAID STEEL BARS SHALL BE MAINTAINED RIGIDLY IN PROPER ALIGNMENT BY APPROVED MEANS.

ALL MAN-HOLE FRAMES AND OTHER FIXED OBJECTS, WITH THE EXPANSION OF FRAMES OTHERWISE MENTIONED IN THOSE SPECIFICATIONS, SHALL BE SEPARATED FROM THE CONCRETE PAVEMENT BY JOINT FILLER OF THE SAME THICKNESS AND CHARACTER AS HEREBEFORE SPECIFIED FOR EXPANSION JOINTS, WHICH JOINT FILLER SHALL EXTEND THROUGH THE ENTIRE THICKNESS OF THE PAVEMENT. SAID JOINT FILLER SHALL BE PLACED IN SUCH A MANNER AS TO FORM A SQUARE WHOSE SIDES WILL BE FIFTEEN INCHES (15") FROM THE UPPER OUTSIDE EDGES OF THE FRAMES.

FURNISHING ALL LABOR, EQUIPMENT, AND MATERIAL FOR INSTALLATION OF SAID EXPANSION JOINTS, LOAD TRANSFER DEVICES AND REINFORCING STEEL SHALL NOT BE PAID FOR DIRECTLY, BUT WILL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION OF THE PORTLAND CEMENT CONCRETE PAVEMENT.

CONTRACTION JOINT

SEE THE DETAIL FOR CONTRACTION JOINT. ALL SAW JOINT TO BE FILLED FLUSH WITH SEALER.

MIXING AND PLACING CONCRETE

CONCRETE SHALL BE MIXED AND PLACED IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 420 OF THE 'STANDARD SPECIFICATIONS'. INSOFAR AS THE SAME MAY APPLY, WHILE BEING PLACED, THE CONCRETE SHALL BE VIGOROUSLY VIBRATED BY THE USE OF A MECHANICAL VIBRATING DEVICE, AND SPADED TO ELIMINATE VOIDS OR HONEYCOMB POCKETS AND BULKHEADS. THE CONCRETE SHALL BE STRUCK OFF, SO THAT THE UPPER SURFACE SHALL CONFORM TO AND BE SUFFICIENTLY HIGHER THAN THE CONTOUR OF THE FINISHED GRADE OF THE UPPER SURFACE OF THE FINISHED PAVEMENT, TO GIVE THE SPECIFIED THICKNESS OF PAVEMENT HEREIN REQUIRED. THE STRIKE BOARD SHALL BE MOVED FORWARD WITH A COMBINED LONGITUDINAL AND TRANSVERSE MOTION, AND SO MANIPULATED AS TO TAMP THE CONCRETE THOROUGHLY. A SLIGHT EXCESS OF MATERIAL SHALL BE KEPT IN FRONT OF THE CUTTING EDGE AT ALL TIMES. THE ENTIRE AREA OF THE PAVEMENT SHALL BE STRUCK OFF AND TAMPED, SO EXECUTED AS TO INSURE MAXIMUM COMPACTON.

FINISHED CONCRETE

AFTER THE VIBRATING AND THE TAMPING HAS BEEN COMPLETED, THE PAVEMENT SHALL BE RUBBED SMOOTH OF ALL RIDGES AND DEPRESSIONS AND BROUGHT TO A TRUE EVEN SURFACE BY THE USE OF WOODEN FLOATS. A SPLIT FLOAT, SHAPED ON THE UNDER SIDE TO CONFORM TO THE CONTOUR OF THE PAVEMENT, SHALL BE USED FOR FINISHING THE CONCRETE ON BOTH SIDES OF THE LONGITUDINAL JOINTS, HEREBEFORE SPECIFIED. THE EDGES ADJOINING THE TRANSVERSE EXPANSION JOINT AND THE FORM SHALL BE TOOLED WITH AN EDGING TOOL HAVE A RADIUS OF ONE-QUARTER INCH (3/4").

AFTER THE FLOATING HAS BEEN COMPLETED, THE SURFACE OF THE PAVEMENT SHALL BE TESTED WITH A NOTCHED STRAIGHT EDGE, TEN FEET (10') IN LENGTH. THIS STRAIGHT EDGE SHALL BE PLACED OVER THE TRANSVERSE JOINTS AND BETWEEN THE SAME AT FREQUENT INTERVALS, PARALLEL TO THE CENTER LINE OF THE PAVEMENT AND MOVED ACROSS FROM SIDE TO SIDE. ANY VIBRATION IN EXCESS OF ONE-EIGHTH INCH (1/8") ABOVE OR BELOW THE GENERAL ESTABLISHED CONTOUR OF THE PAVEMENT SHALL BE RUBBED WITH A LONG HANDLED FLOAT AND THE IRREGULARITIES CORRECTED UNTIL THE STRAIGHT EDGE SHALL SHOW A BEARING FOR ITS ENTIRE LENGTH.

WORKMEN SHALL NOT WALK ON FRESHLY LAID CONCRETE AND NO ONE SHALL BE ALLOWED ON THE CONCRETE AFTER IT HAS BEEN WORKED INTO FORM AND SHAPE UNTIL IT HAS ENTIRELY HARDENED. THE ENTIRE SURFACING OF THE CONCRETE PAVEMENT SHALL BE DONE WITH A BRIDGE, NOT LESS THAN ONE FOOT (1') IN WIDTH WHICH SHALL NOT COME IN CONTACT WITH THE CONCRETE PAVEMENT AT ANY POINT. TWO (2) OF SAID BRIDGES SHALL BE PROVIDED FOR EACH CONCRETE MIXING GANG. TWO BRIDGES SHALL BE MAINTAINED IN CONSTANT READINESS TO BE IMMEDIATELY MOVED INTO POSITION, TO PROVIDE ACCESS FOR FINISHING, TOOLING AND EDGING JOINTS, ADJUSTING AND REFINISHING DAMAGED PORTIONS OF THE FINISHED SURFACE.

PROTECTION AND CURING

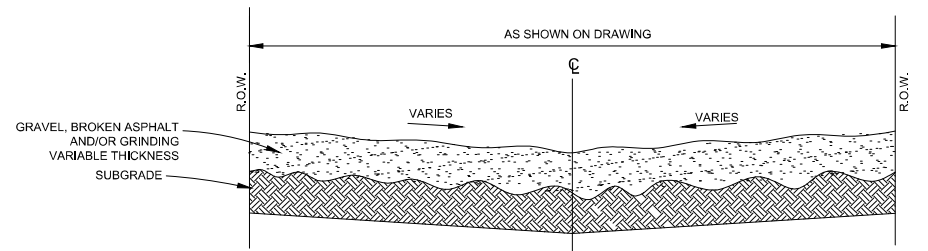
PROTECTION AND CURING SHALL COMPLY WITH THE REQUIREMENTS OF SECTION 420 OF THE 'STANDARD SPECIFICATIONS'.

PROTECTIVE SEALER

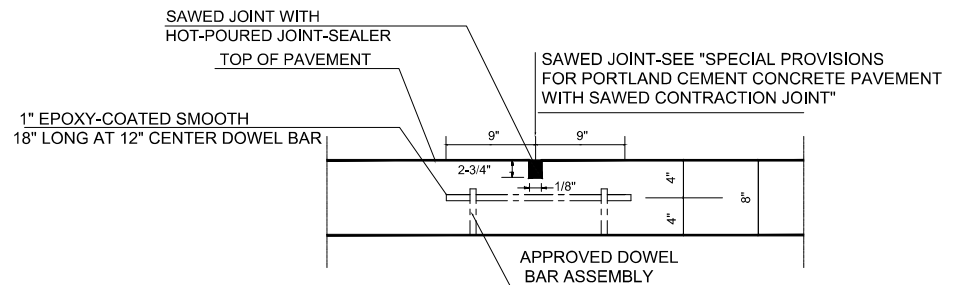
TO REDUCE THE PENETRATION OF SALT, THE NUMBER ONE CAUSE OF REPAIR CORROSION, SPALLING AND CRACKING, A PROTECTIVE SURFACE TREATMENT CONSISTING OF BSM 20 OR EQUAL SHALL BE APPLIED TO THE SURFACE OF THE PORTLAND CEMENT CONCRETE PAVEMENT. BEFORE THE SURFACE TREATMENT MIXTURE IS APPLIED, THE CONCRETE SURFACE SHALL HAVE A MINIMUM CURING PERIOD OF 28 CALENDAR DAYS PRIOR TO THE APPLICATION OF THE MIXTURE AND SHALL BE CLEANED TO REMOVE ALL OIL, GRIME, AND LOOSE PARTICLES WHICH WOULD PREVENT THE MIXTURE FROM PENETRATING THE CONCRETE. IF CHEM-TRETE BSM 20 WILL BE USED FOR PROTECTIVE SURFACE TREATMENT, SODIUM SILICATE CURING COMPOUND SHALL BE APPLIED FOR CONCRETE CURING, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE TEMPERATURE OF CONCRETE AND THE AIR SHALL BE 50 DEG. F OR HIGHER AT THE TIME OF APPLICATION.

SUMMARY OF QUANTITIES
PROJECT NO.: SA 1531
PAVING OF ALLEY NORTH OF COLFAX STREET, EAST OF BRYANT AVENUE

Table with 4 columns: ITEM NO., ITEM, UNIT, QUANTITY. Lists 35 items including earth excavation, trench backfill, sub-base granular material, aggregate base course, incidental hot-mix asphalt surfacing, storm sewers, curb and gutter, concrete curb, and various paving and marking materials.

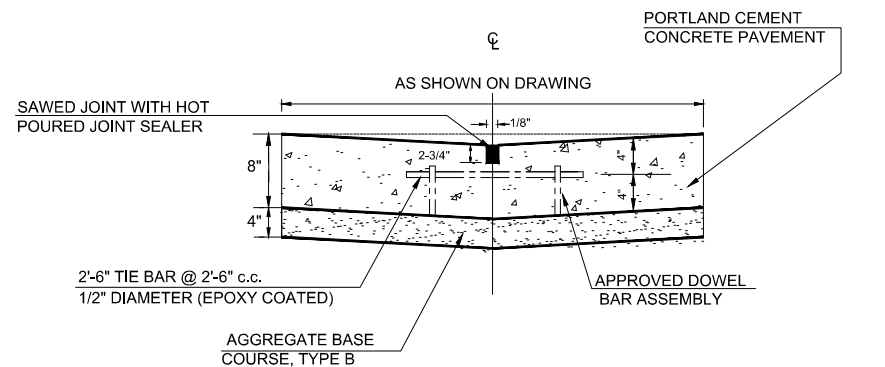


EXISTING TYPICAL CROSS-SECTION (NOT TO SCALE)



SAWED CONTRACTION JOINT

ALL SAWED JOINT TO BE FILLED FLUSH WITH SEALER, HOT Poured TYPE FOR JOINTS IN CONCRETE S.S.-5-164, OR WITH COLD APPLIED READY MIXED CONCRETE JOINT SEALING COMPOUND AS SPECIFIED IN THE "SPECIAL PROVISION FOR PORTLAND CEMENT CONCRETE PAVEMENT WITH SAWED CONTRACTION JOINTS"



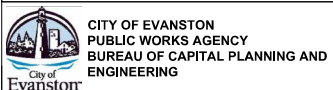
LONGITUDINAL SAWED CENTER JOINT TYPICAL SECTION

Table titled 'ELEVATION BENCHMARKS DATUM: CITY OF EVANSTON' with columns for NO., DESCRIPTION, and ELEV. Lists benchmarks such as Northwest Bolt of Fire Hydrant at 20.35 and Tag Bolt of Fire Hydrant at 21.53.

ALLEY N OF COLFAX ST, E OF BRYANT AVE
GENERAL NOTES, SOQ, AND TYPICAL SECTIONS

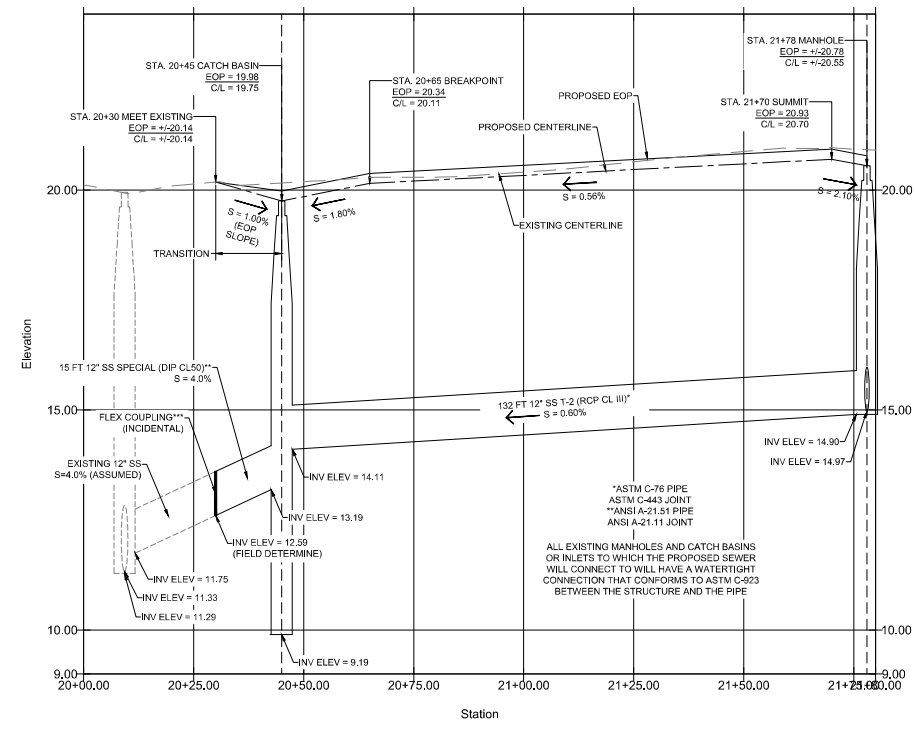
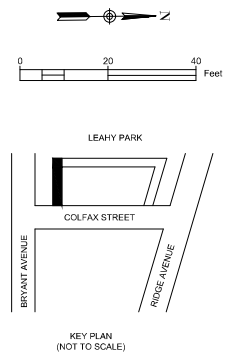
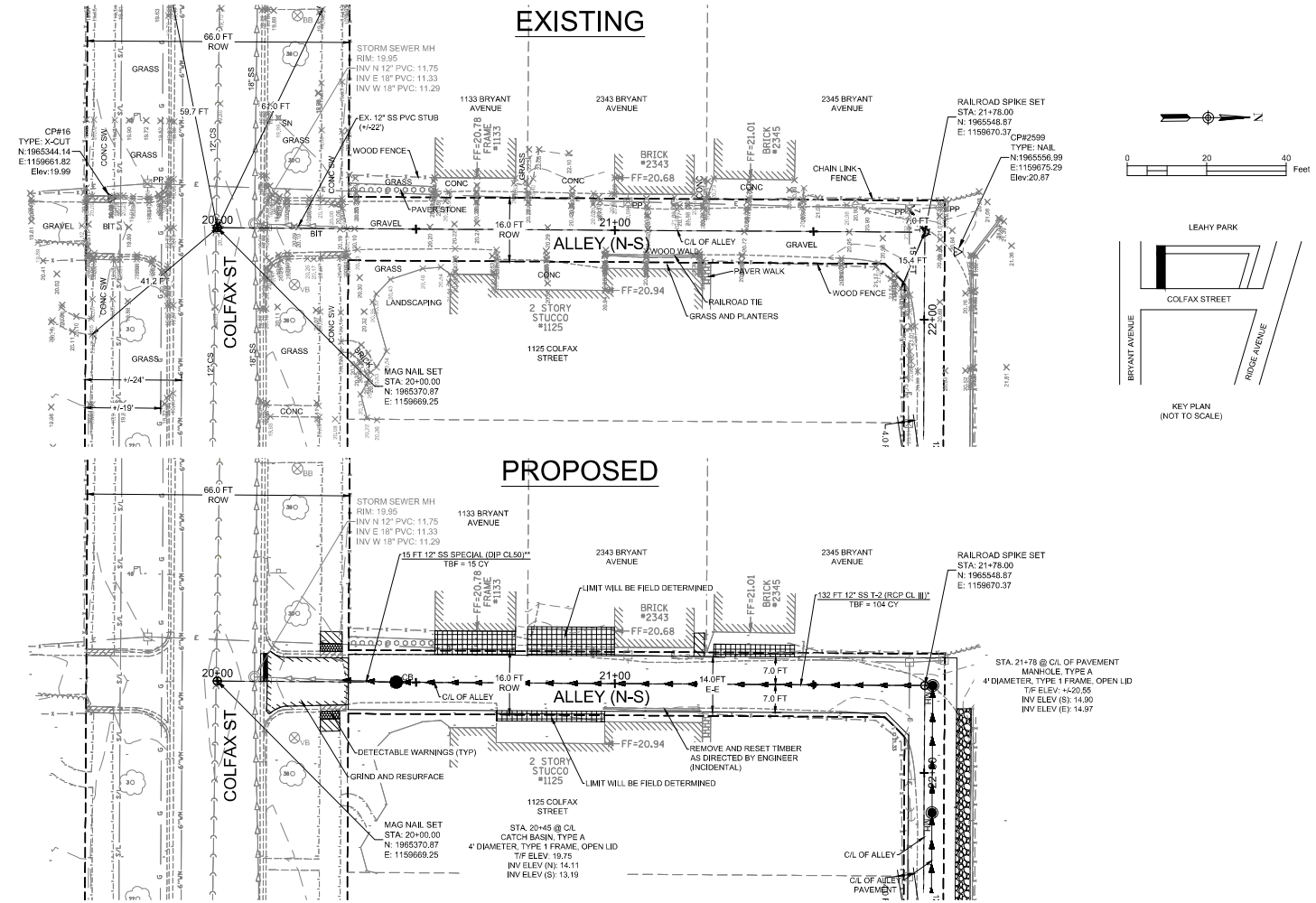
Table with project information including Scale (Horizontal N/A, Vertical N/A), Project Number (SA 1531), Bid Number (24-XX), Issued For (Review), Date (02/14/2024), and Designers (PC, BN).

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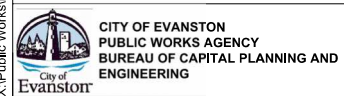
Revision table with columns for NO. and DATE. Shows revision 4 and revision 3.





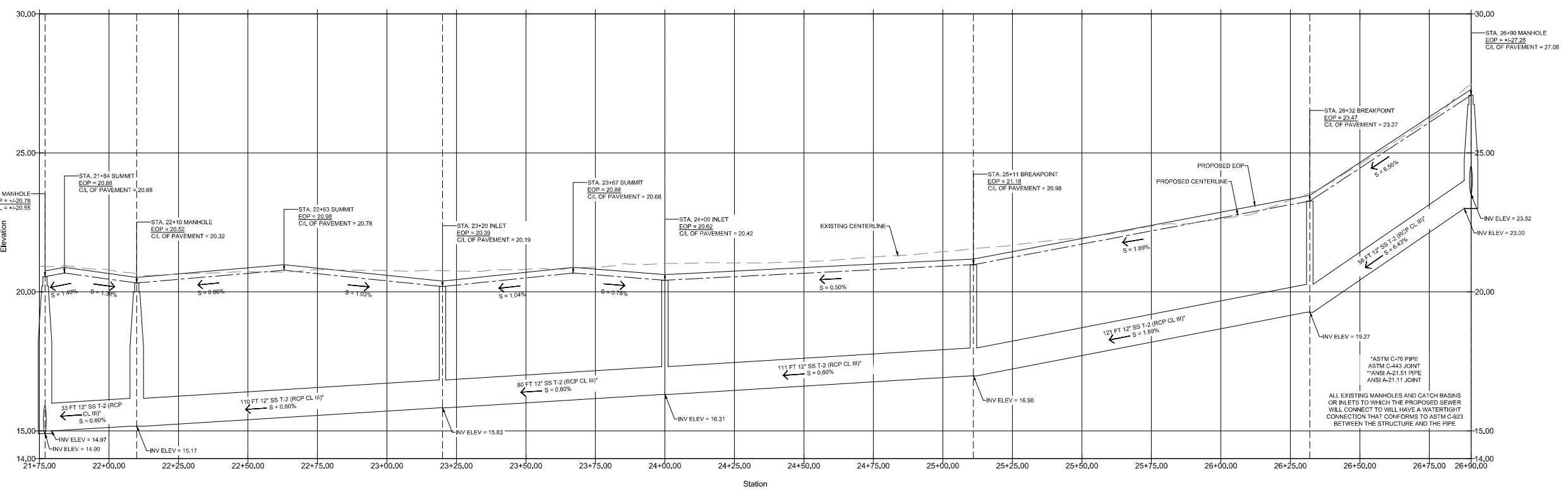
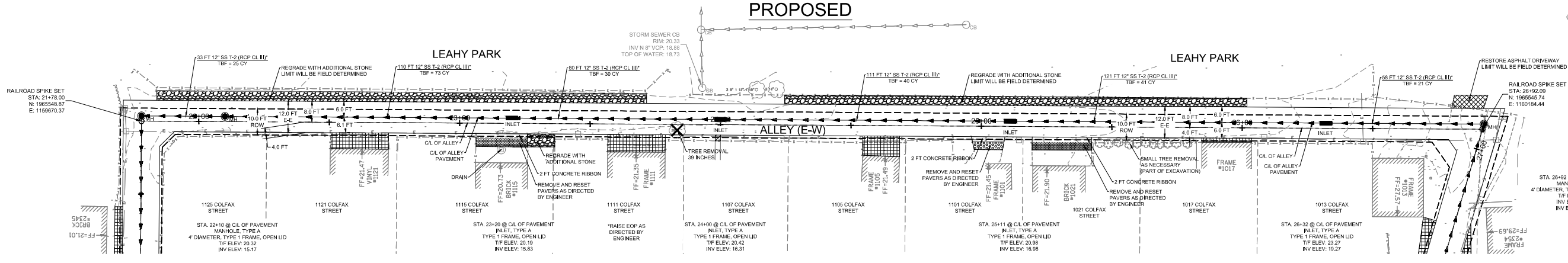
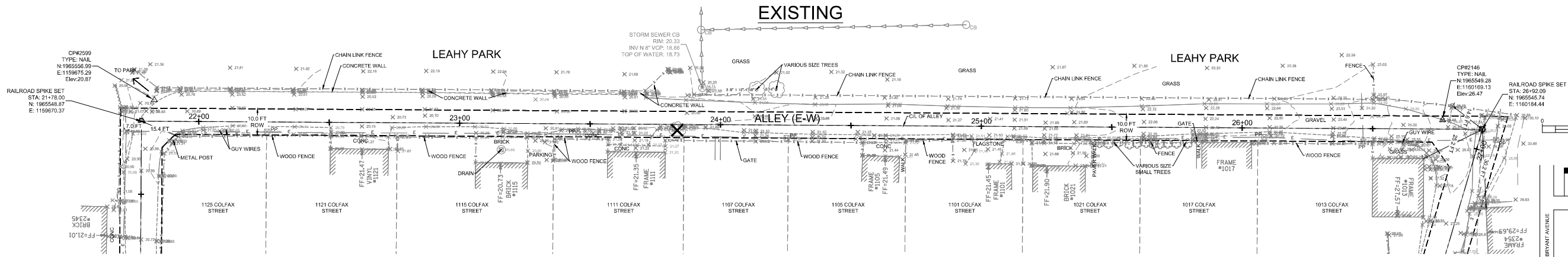
## ALLEY N OF COLFAX ST, E OF BRYANT AVE PLAN AND PROFILE

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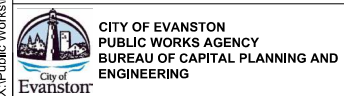


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SCALE	HORIZONTAL	1" = 20'	PROJECT NUMBER: SA 1531	ISSUED FOR: REVIEW	DESIGNED BY: PC	SHEET
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			BID NUMBER: 24-XX	DATE: 02/14/2024	DRAWN BY: BN	3 OF 7
					CHECKED BY: PC, BN	



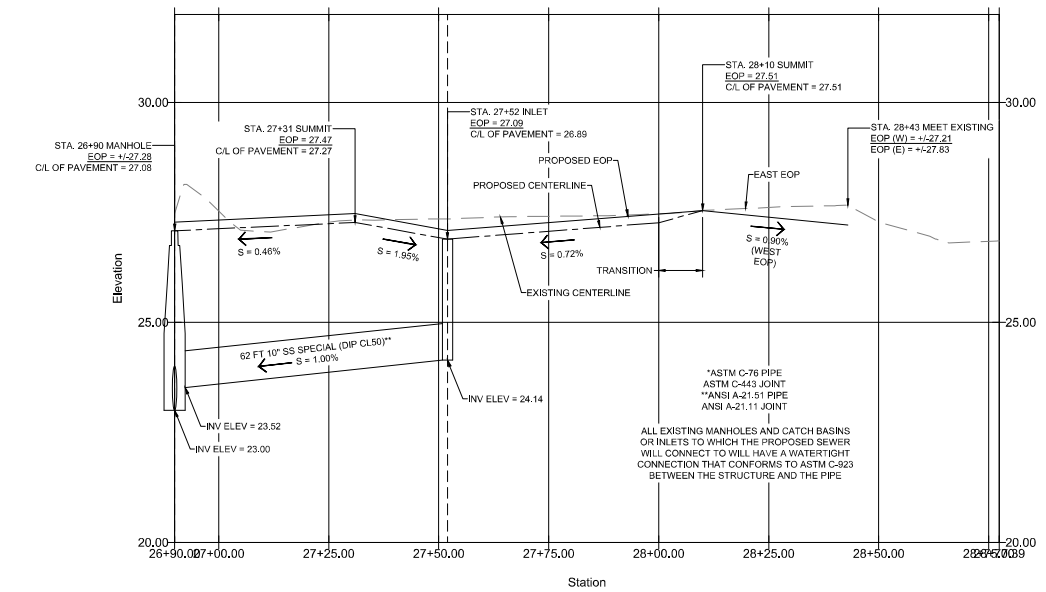
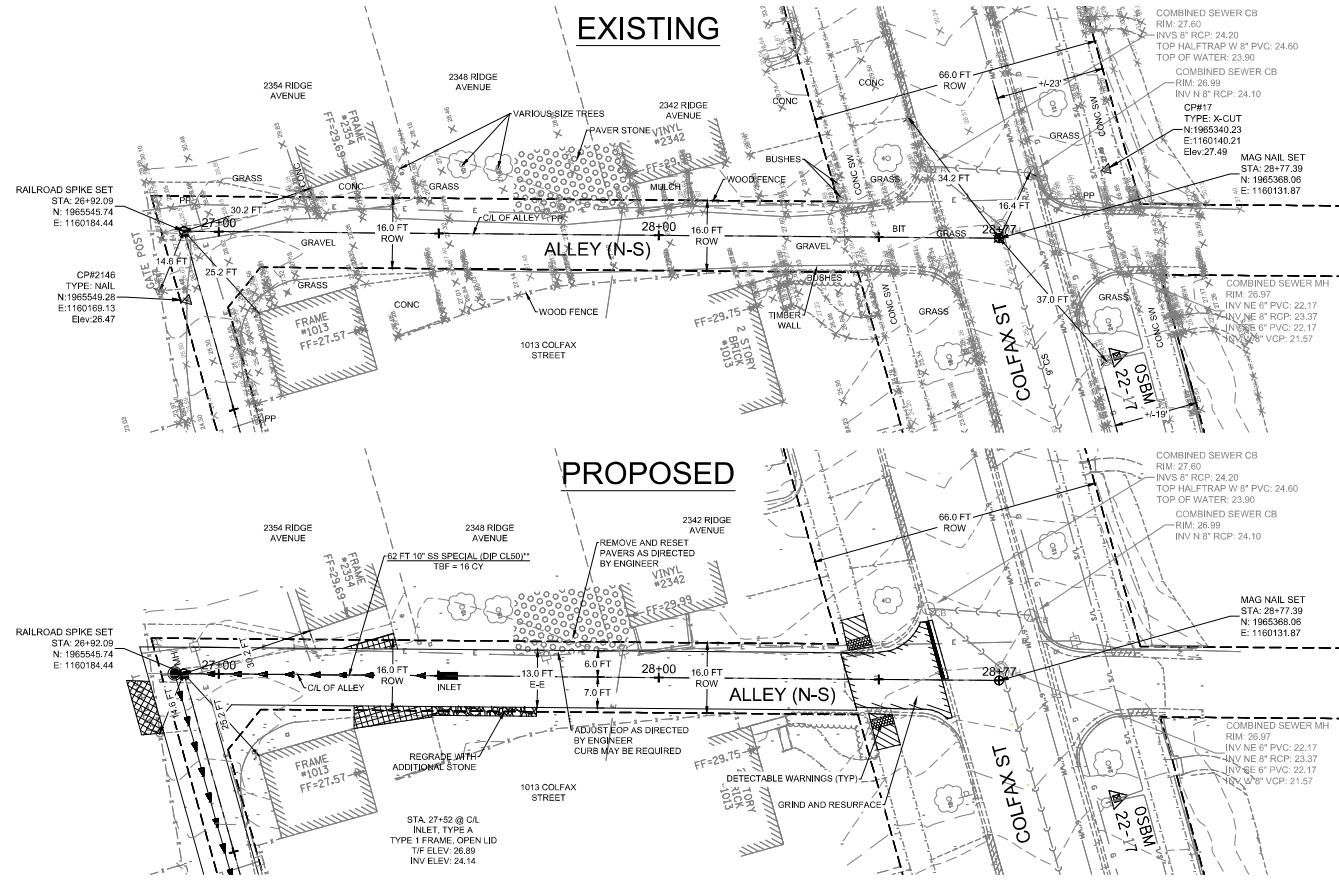
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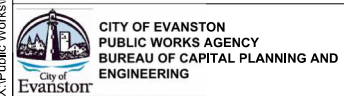
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## ALLEY N OF COLFAX ST, E OF BRYANT AVE PLAN AND PROFILE

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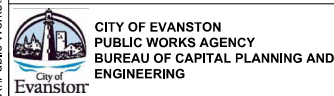
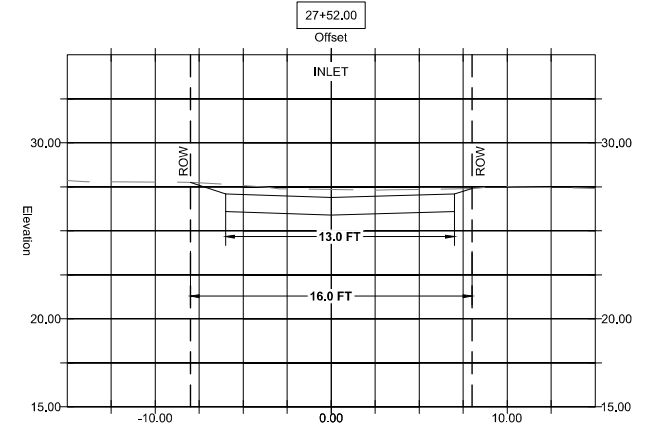
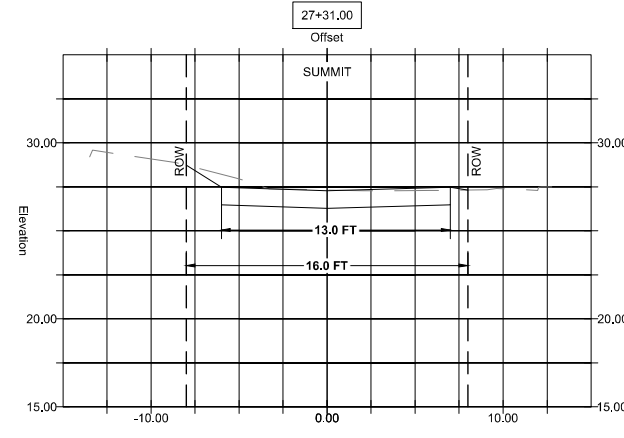
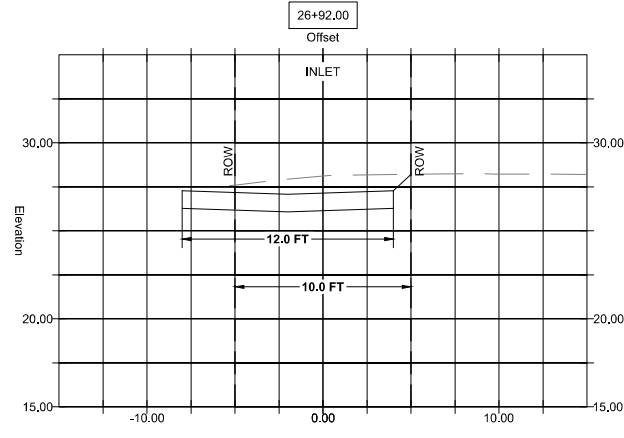
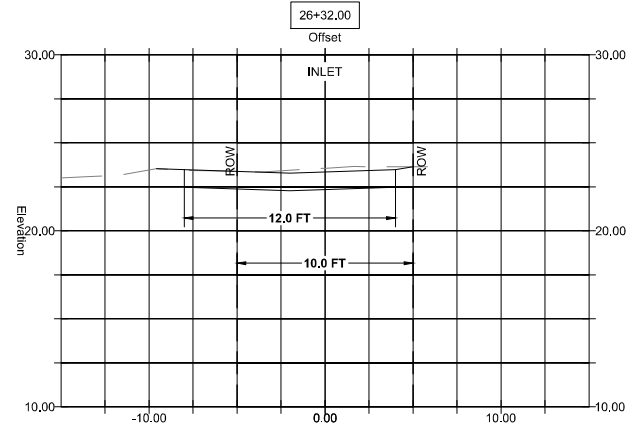
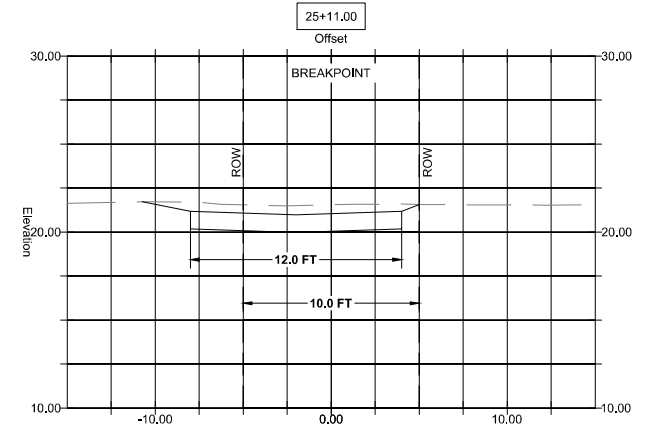
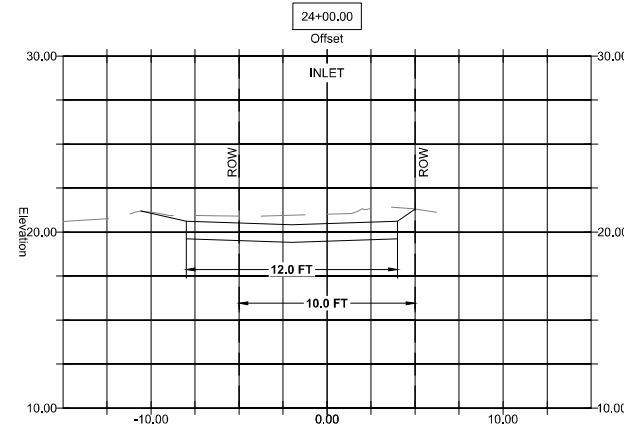
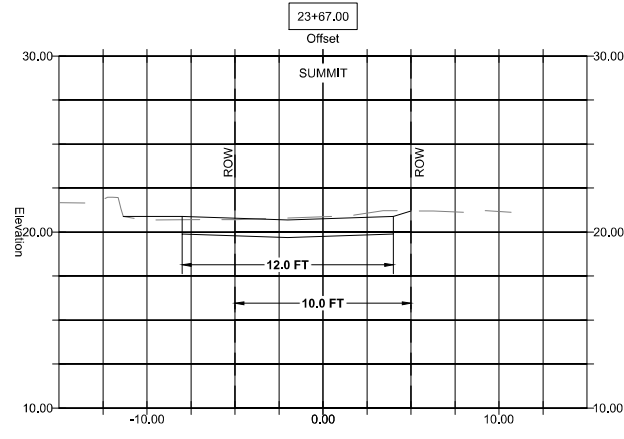
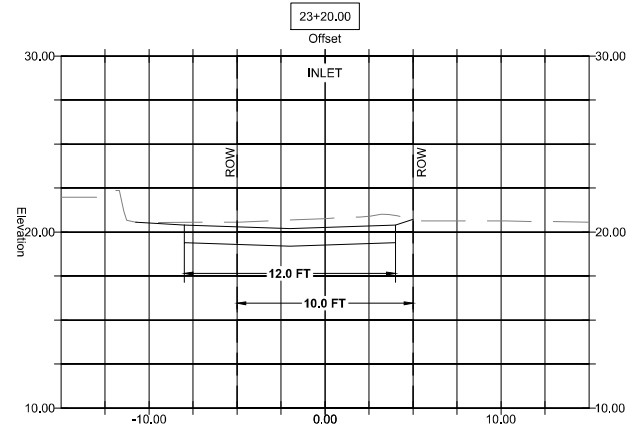
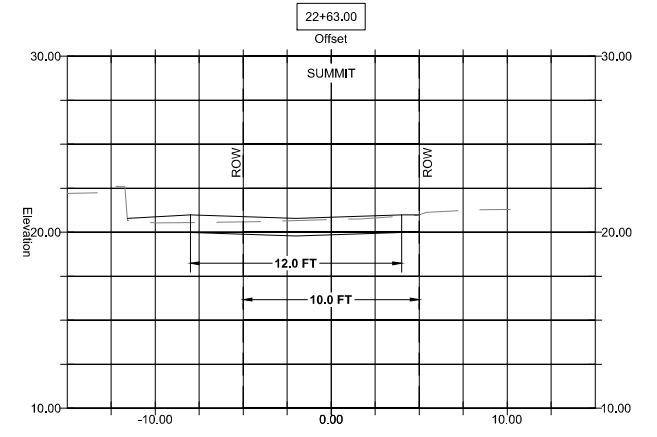
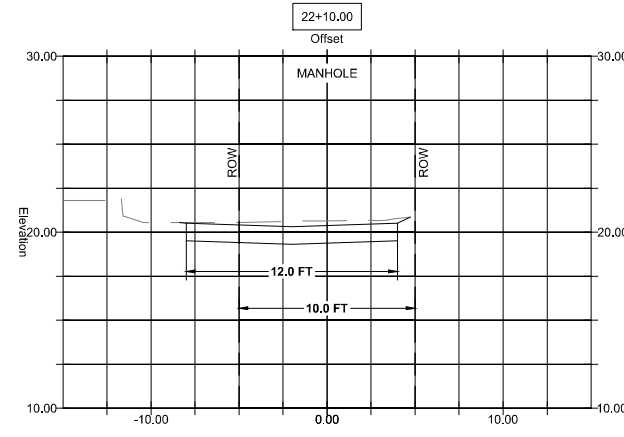
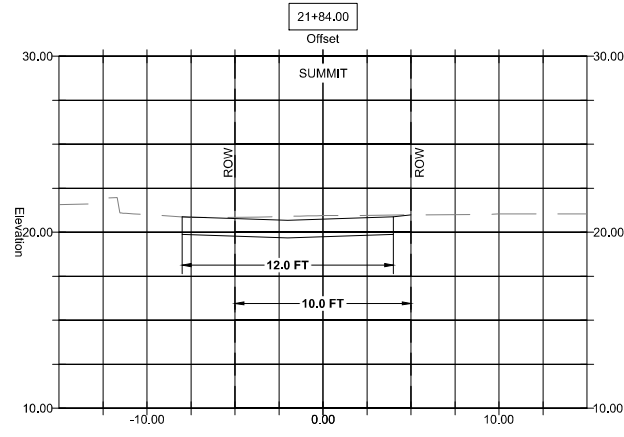
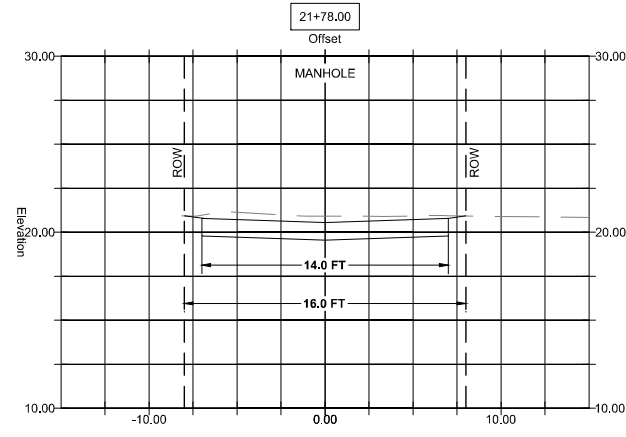
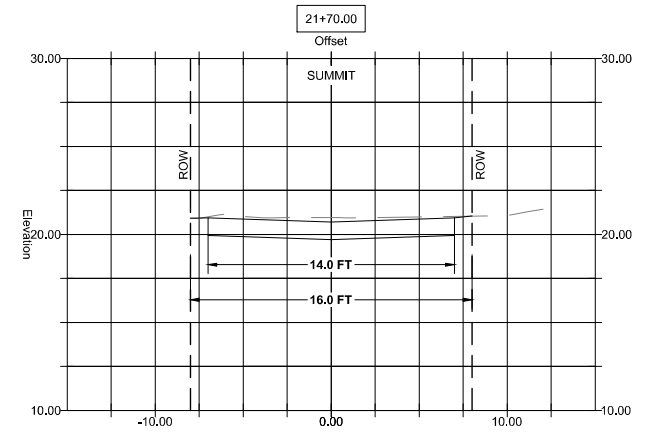
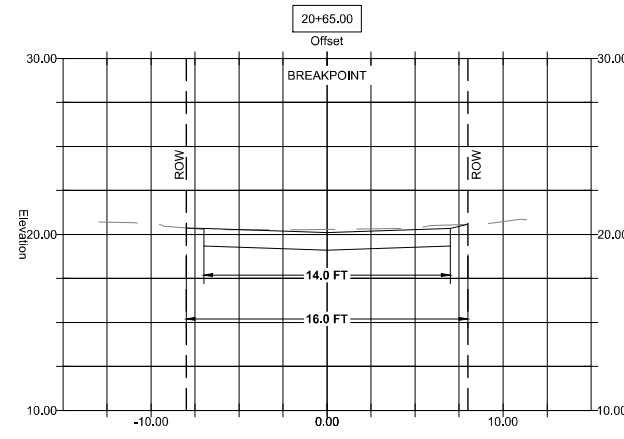
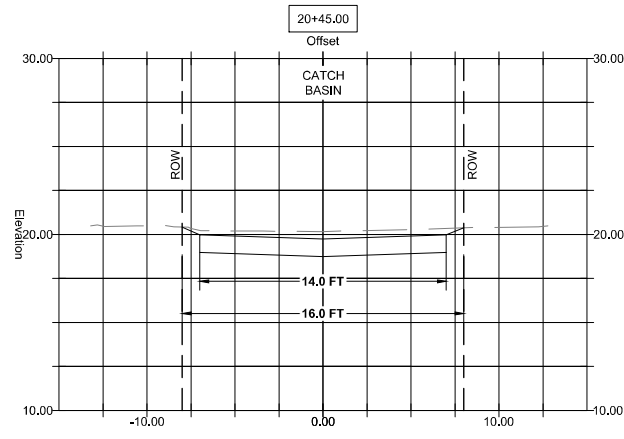
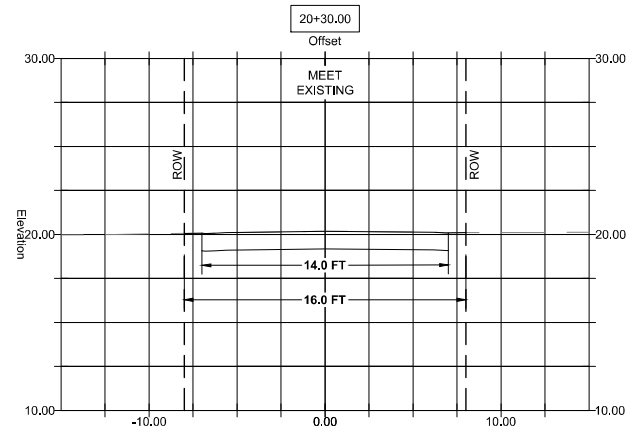


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DATE	NO.	REVISION

## ALLEY N OF COLFAX ST, E OF BRYANT AVE PLAN AND PROFILE

SCALE	HORIZONTAL	1" = 20'	PROJECT NUMBER: SA 1531	ISSUED FOR: REVIEW	DESIGNED BY: PC	SHEET				
	VERTICAL	1" = 2'					BID NUMBER: 24-XX	DATE: 02/14/2024	DRAWN BY: BN	5 OF 7

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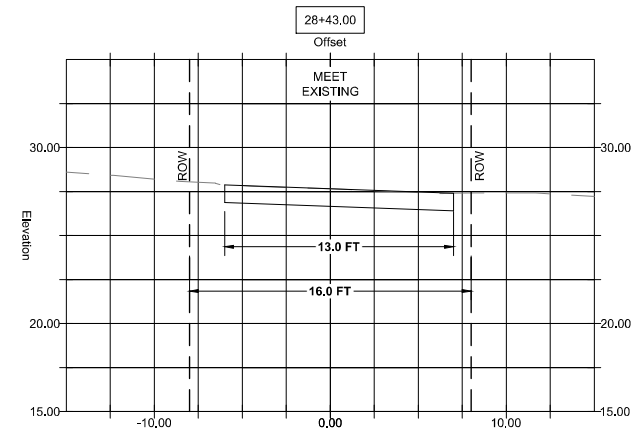
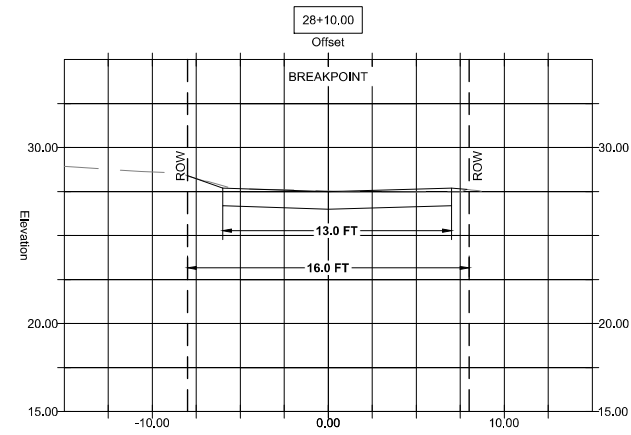
## ALLEY N OF COLFAX ST, E OF BRYANT AVE CROSS SECTIONS

SCALE
HORIZONTAL 1" = 5'
VERTICAL 1" = 5'

PROJECT NUMBER: SA 1531
BID NUMBER: 24-XX

ISSUED FOR: REVIEW
DATE: 02/14/2024

DESIGNED BY: PC
DRAWN BY: BN
CHECKED BY: PC, BN



AT ALL UTILITY POLES AND OTHER ENCROACHMENTS, EOP AND CONCRETE CURB ALONG EOP WILL BE FIELD DETERMINED

PREPARATION OF BASE: ALL LABOR, EQUIPMENT, AND MATERIAL NECESSARY FOR BASE PREPARATION SHALL BE INCLUDED IN VARIOUS PAY ITEMS. THIS WORK WILL NOT BE PAID FOR SEPARATELY.

EARTH EXCAVATION = 466.22 X 1.03 = 480 CUBIC YARDS

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	4	
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DATE	NO.	REVISION

## ALLEY N OF COLFAX ST, E OF BRYANT AVE CROSS SECTIONS

SCALE		PROJECT NUMBER: SA 1531	ISSUED FOR: REVIEW	DESIGNED BY: PC
HORIZONTAL	1" = 5'	BID NUMBER: 24-XX	DATE: 02/14/2024	DRAWN BY: BN
VERTICAL	1" = 5'			CHECKED BY: PC, BN

# ESTIMATE OF ENGINEER OF BOARD OF LOCAL IMPROVEMENTS

To the Board of Local Improvements of the City of Evanston:

Pursuant to a Resolution of the Board of Local Improvements of the City of Evanston, heretofore adopted, providing that the unimproved public alley in the block bounded by Leahy Park on the north, Colfax Street on the south, Bryant Avenue on the west, and Ridge Avenue on the east, in the City of Evanston, be improved by excavating, constructing the necessary drainage system and paving with 8" Portland Cement Concrete Pavement, to a width of twelve feet (12'), West North South leg to a width of fourteen feet (14') and East North South leg to a width of thirteen feet (13').

Said improvement to be constructed in detail as specified in aforesaid Resolution.

I, herewith submit an estimate of cost of such improvement as described in said Resolution, including labor, materials, and all other lawful expenses attending the proceedings for making said improvement and the cost of making and collecting the assessment therefore as provided by law,

## COST ESTIMATE SPECIAL ASSESSMENT NO: 1531

<u>ITEM NO.</u>	<u>ITEM</u>	<u>UNIT</u>	<u>QUANTITY</u>	<u>UNIT PRICE</u>	<u>TOTAL</u>
1	EARTH EXCAVATION	CU. YD.	480	\$60.00	\$28,800.00
2	TRENCH BACKFILL	CU. YD.	365	\$38.00	\$13,870.00
3	SUB-BASE GRANULAR MATERIAL, TYPE B	CU. YD.	15	\$38.00	\$570.00
4	GRANULAR CRADLE MATERIAL, CA-11	TON	275	\$36.00	\$9,900.00
5	AGGREGATE BASE COURSE, TYPE B, 4"	SQ. YD.	1,140	\$8.00	\$9,120.00
6	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	14	\$300.00	\$4,200.00
7	PORTLAND CEMENT CONCRETE PAVEMENT, 8"	SQ. YD.	1,135	\$110.00	\$124,850.00
8	STORM SEWERS, SPECIAL, 10" ( D. I. P. / CL 50 )	FOOT	62	\$145.00	\$8,990.00
9	STORM SEWERS, 12", TYPE 2 ( R. C. P. / CL III )	FOOT	645	\$135.00	\$87,075.00
10	STORM SEWERS, SPECIAL 12" (D.I.P. /CL 50)	FOOT	15	\$190.00	\$2,850.00
11	CATCH BASIN, TYPE A, 4' DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	\$6,000.00	\$6,000.00
12	MANHOLE, TYPE A, 4' DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	3	\$5,400.00	\$16,200.00
13	INLET, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	5	\$3,000.00	\$15,000.00
14	COMBINATION CONCRETE CURB AND GUTTER, TYPE B 6.12	FOOT	35	\$38.00	\$1,330.00
15	CONCRETE CURB , TYPE B	FOOT	180	\$38.00	\$6,840.00
16	PAVEMENT REMOVAL	SQ. YD.	10	\$26.00	\$260.00
17	DRIVEWAY PAVEMENT REMOVAL	SQ. YD.	180	\$21.00	\$3,780.00
18	CURB REMOVAL	FOOT.	215	\$7.00	\$1,505.00
19	SIDEWALK REMOVAL	SQ. FT.	310	\$3.00	\$930.00
20	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT - 6"	SQ. YD.	160	\$78.00	\$12,480.00
21	PORTLAND CEMENT CONCRETE SIDEWALK - 5"	SQ. FT.	310	\$11.00	\$3,410.00
22	PROTECTIVE SEALER	SQ. YD.	1,295	\$1.60	\$2,072.00
23	CRUSHED STONE, CA - 14	TON	30	\$35.00	\$1,050.00
24	FURNISHING AND PLACING TOPSOIL, 3"	SQ. YD.	120	\$10.00	\$1,200.00
25	SODDING, SALT TOLERANT	SQ. YD.	120	\$18.00	\$2,160.00
26	DETECTABLE WARNINGS	SQ.FT..	40	\$55.00	\$2,200.00



Estimate of Engineer BLI – S.A. 1531  
 (Alley N/Colfax St., E/Bryant Ave.)

<u>ITEM NO.</u>	<u>ITEM</u>	<u>UNIT</u>	<u>QUANTITY</u>	<u>UNIT PRICE</u>	<u>TOTAL</u>
27	TREE REMOVAL (OVER 15 INCH DIA)	UNIT. DIA.	39	\$125.00	\$4,875.00
28	HOT-MIX ASPHALT SURFACE REMOVAL	SQ. FT.	90	\$30.00	\$2,700.00
29	TEMPORARY FENCE	FOOT.	150	\$7.00	\$1,050.00
30	CONSTRUCTION LAYOUT AND STAKING	LUMP SUM	1	\$7,500.00	\$7,500.00
31	STREET SWEEPING	EACH	3	\$400.00	\$1,200.00
32	CLOSED CIRCUIT TV INSPECTION	LUMP SUM	1	\$5,500.00	\$5,500.00
33	PRE-CONSTRUCTION SURFACE VIDEO TAPING	LUMP SUM	1	\$2,000.00	\$2,000.00
34	REMOVING AND RESETTING PAVERS	SQ. FT.	550	\$25.00	\$13,750.00
35	EPOXY PAVEMENT MARKING LINE- 6" YELLOW	FOOT	115	\$40.00	\$4,600.00
				<b>TOTAL</b>	<b>\$409,817.00</b>

Cost of Engineering Services.....	\$18,441.77
For Lawful Expenses, etc.....	\$18,441.77
Sub-Total .....	<b>\$36,883.54</b>

**RECAPITULATION**

Construction Cost.....	\$409,817.00
Engineering.....	\$18,441.77
Legal.....	\$18,441.77
<b>Total Cost.....</b>	<b>\$446,700.54</b>

I hereby certify that in my opinion the above estimate does not exceed the probable cost of the above proposed improvement and the lawful expenses attending the same.

\_\_\_\_\_  
 Lara N. Biggs, P.E.  
 City Engineer  
 Board of Local Improvements

ALLEY NORTH OF COLFAX STREET  
EAST OF BRYANT AVENUE

BOARD OF LOCAL IMPROVEMENTS  
ASSESSMENT ROLL  
SPECIAL ASSESSMENT NO. 15

UNIT METHOD

ADDRESS (OWNER)	ADDRESS (SITE)	PERMANENT REAL ESTATE INDEX NO.	PART OF LOT OR LAND	LEGAL		NO OF UNITS	ASSESSMENT PER UNIT	ASSESSMENT			
				LOT	BLOCK			1st ASSESSMENT	2-10 INSTALLMENT	TOTAL	
1130 GREEN LANE AVE WILMETT, IL 60091 R.C. BROWN'S SUBDIVISION IN THE WEST 1/2 OF THE SOUTHWEST 1/4 OF SECTION 7, TOWNSHIP 41 NORTH, RANGE 14 EAST OF THE THIRD PRINCIPAL MERIDIAN IN COOK COUNTY, ILLINOIS.											
RICHARD J. SWEITZ OR CURRENT OWNER	2904 RIDGE AVENUE EV. IL. 60201	11-07-108-004-0000	PART OF	4			1	862.86	853.00	8,539.86	
GEORGIE A. PITTS OR CURRENT OWNER	2345 BRYANT AVE EV. IL. 60201	11-07-108-005-0000	ALL OF	13			1	862.86	853.00	8,539.86	
TOBI GITA EHRENPREIS OR CURRENT OWNER	2343 BRYANT AVE EV. IL. 60201	11-07-108-006-0000	ALL OF	14			1	862.86	853.00	8,539.86	
IAN LOW OR CURRENT OWNER	1133 COLFAX STREET EV. IL. 60201	11-07-108-007-0000	ALL OF	15			1	862.86	853.00	8,539.86	
ANDREW CARLO OR CURRENT OWNER	1125 COLFAX STREET EV. IL. 60201	11-07-108-008-0000	ALL OF	50			1	862.86	853.00	8,539.86	
PAUL J. TAKACS OR CURRENT OWNER	1121 COLFAX STREET EV. IL. 60201	11-07-108-009-0000	ALL OF PART OF	49 48			1	862.86	853.00	8,539.86	
JEROME J. WARD OR CURRENT OWNER	1115 COLFAX STREET EV. IL. 60201	11-07-108-010-0000	PART OF PART OF	48 47			1	862.86	853.00	8,539.86	
CATHERINE SARDO WEIDNE OR CURRENT OWNER	1111 COLFAX STREET EV. IL. 60201	11-07-108-011-0000	PART OF ALL OF	47 46			1	862.86	853.00	8,539.86	
TIARA WINSTON OR CURRENT OWNER	1107 COLFAX STREET EV. IL. 60201	11-07-108-012-0000	ALL OF	45			1	862.86	853.00	8,539.86	
GREGORY M. SCHDEFFMAN OR CURRENT OWNER	1105 COLFAX STREET EV. IL. 60201	11-07-108-013-0000	ALL OF	44			1	862.86	853.00	8,539.86	
ASWIN A VAN DEN BERG OR CURRENT OWNER	1101 COLFAX STREET EV. IL. 60201	11-07-108-014-0000	ALL OF	43			1	862.86	853.00	8,539.86	

ADDRESS (OWNER)	ADDRESS (SITE)	PERMANENT REAL ESTATE INDEX NO	PART OF LOT OR LAND	LEGAL		NO OF UNITS	ASSESSMENT PER UNIT	ASSESSMENT		
				LOT	BLOCK			1st ASSESSMENT	2-10 INSTALLMENT	TOTAL
RAYMOND P. WEXLER OR CURRENT OWNER	1021 COLFAX STREET EV. IL. 60201	11-07-108-015-0000	ALL OF	42		1		862.87	853.00	8,539.87
ANDREW JOSEPH SHOOT OR CURRENT OWNER	1017 COLFAX STREET EV. IL. 60201	11-07-108-016-0000	ALL OF	41		1		862.87	853.00	8,539.87
C&K VANAVERMA ETE OR CURRENT OWNER	1013 COLFAX STREET EV. IL 60201	11-07-108-017-0000	ALL OF	40		1		862.87	853.00	8,539.87
DIANNE RUCINSKI OR CURRENT OWNER	2354 RIDGE AVE. EV. IL. 60201	11-07-108-018-0000	ALL OF	39		1		862.87	853.00	8,539.87
LOUISE E. TAYLOR OR CURRENT OWNER	2348 RIDGE AVE. EV. IL. 60201	11-07-108-019-0000	ALL OF	38		1		862.87	853.00	8,539.87
CHRISTINE BRIDGER OR CURRENT OWNER	2342 RIDGE AVE EV. IL 60201	11-07-108-020-0000	ALL OF	37		1		871.87	852.00	8,539.87
						TOTAL	17	14,677.68	14,500.00	145,177.68
CONSTRUCTION COST	: \$409,817.00	Actual Benefit with City contribution:  CITY CONTRIBUTION: 223,350.27 + 78,172.59 = 301,522.86  PRIVATE BENEFIT: 145,177.68  35% of Half - 78,172.59								
ENGINEERING	: 18,441.77									
LEGAL	: 18,441.77									
TOTAL COST OF IMPROVEMENT	: \$446,700.54									
COST PER RUNNING FOOT	:									

TOTAL:

COST PER UNIT: \$