

Section 3: Master Plan

The Future Central Street

The Master Plan provides a vision for how Central Street can be enhanced as a unique mixed-use, neighborhood-oriented district that includes residential, retail, office, service, civic, institutional, and transit uses. The Plan is based on the community's desire for higher-quality development and streetscape improvements that maintain the area's eclectic, "funky," "European village," "small town" character.

The following are key positive features and development "building blocks" that are incorporated in the Master Plan:

- Impassioned and committed community residents.
- A location close to Downtown Evanston and other North Shore communities.
- Strong transportation and transit connections.
- The presence of numerous local and regional activity generators.
- A tradition of small-scale, unique retailers.
- Strong developer interest.
- Presence of important civic institutions.
- Well-maintained, desirable residential districts.
- Well-situated and attractive parks and open spaces.
- Presence of underutilized properties.
- Committed leadership.

The following are key Master Plan objectives:

1. Sustain and enhance Central Street as an attractive, mixed-use, pedestrian-oriented street with its own unique character.
2. Sustain and enhance Central Street as a location for diverse, small-scale retail shops, service businesses, and restaurants.
3. Sustain and enhance Central Street as a transit-oriented community with two rail lines and bus service.
4. Sustain and enhance Central Street as an area of well-maintained, vibrant and green neighborhoods.
5. Sustain and enhance Central Street as a local and regional destination for civic, cultural, and athletic facilities.

6. Encourage commercial development, including office uses, in key locations to provide a diverse mix of goods and services to residents and visitors.
7. Enhance existing housing and provide diverse residential opportunities.
8. Improve the appearance and use of existing open space, and the appearance and safety of the area's streetscape.
9. Improve pedestrian, bicycle and vehicular access and circulation.
10. Encourage the use of sustainable building and site design, both in new buildings and in adaptive reuse/renovations/facade improvements.

Strategies

A. Building Envelope

A “building envelope” defines the height, scale and massing of buildings along a street and within a development site. The Master Plan provides concepts for how new buildings can relate to streets and public spaces to help define a more pedestrian-oriented shopping environment.

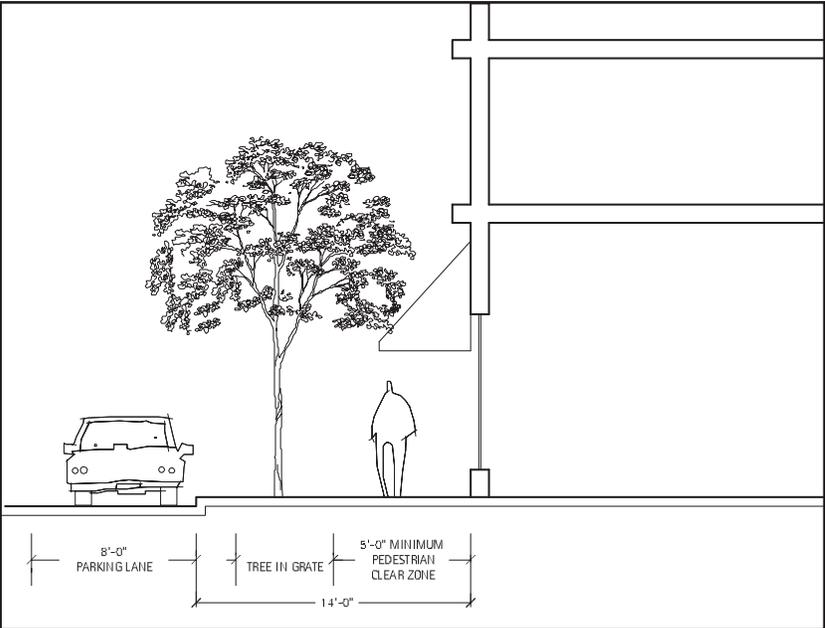
- A1. Setbacks:** New building setbacks should be established to ensure consistent building placement and to create a “streetwall.”
- A2. Commercial Area Setbacks:** In commercial areas along Central Street, buildings should be set back 14 feet from the curb and set back 20 feet from the curb on intersecting side streets. On commercial blocks along Green Bay Road, buildings should be set back 14 feet from the curb.
- A3. Central/Gross Point/Crawford Setbacks:** For properties fronting the intersection of Central, Gross Point and Crawford, buildings should be set back 30 feet from the curb.
- A4. Residential Setbacks:** In residential areas, buildings should continue to be set back 27 feet from the property line (as required by zoning).
- A5. Stepbacks:** Building stepbacks at upper stories should be used to further articulate buildings and reduce the perceived height and mass of new developments along streets and sidewalks.

B. Sidewalks

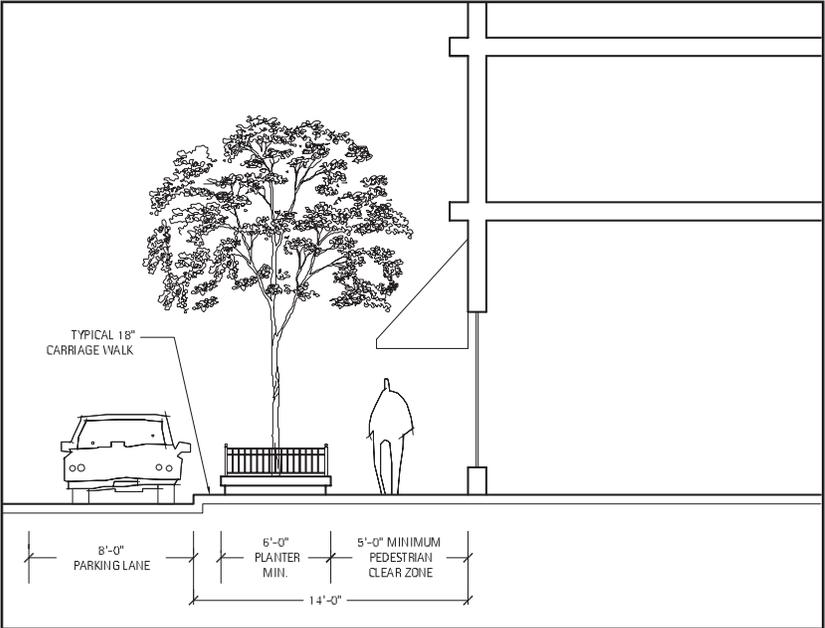
- B1. Sidewalk Standards:** New sidewalk standards should be established for improved sidewalk widths, sight lines, and streetscapes. New developments in commercial areas should provide a minimum 14-foot wide sidewalk with trees in grates or low raised planters. New developments in residential areas should provide a 5-foot wide sidewalk.
- B2: Sidewalk Widening:** Along the commercial blocks west of Lincolnwood Drive, Central Street should be narrowed to allow the addition of 5 feet to the sidewalks on both sides of the street, where feasible. This will provide a minimum 12-foot wide sidewalk.
- B3: Sidewalk Improvements:** Sidewalks should be replaced or constructed wherever they are deteriorated or missing.

Central Street: Master Plan

Section 3: Master Plan



Optimal 14-foot sidewalk cross-section with tree in a tree grate.



Optimal 14-foot sidewalk cross-section with tree in a low planter.

C. Design Guidelines

To complement the City’s “Design Guidelines for Planned Developments,” additional design guidelines should be developed to encourage high-quality development in neighborhood-oriented commercial areas. The guidelines should encourage:

- C1. Building Articulation:** Use projections, recesses, material changes, parapets, cornices, varying roof heights/lines, and/or new facades to avoid long, monolithic structures.
- C2. Avoid Long Buildings:** Large developments that are broken into smaller buildings or articulated to avoid the appearance of long, monolithic structures.
- C3. Defined Facades:** Buildings with clearly defined bases, middles, and tops.
- C4. Special Features:** Use cupolas, atriums, clock towers, and/or varying rooflines for highly visible and prominent corner buildings.
- C5. Stepbacks:** Incorporate stepbacks or upper floor terraces to further articulate buildings.
- C6. Distinct First-Floor Bases:** Use clear glass and kneewalls for retail and mixed-use buildings to easily identify storefronts.
- C7. Retail Depths:** At least 50 feet.
- C8. Consistent Facades:** Visible rears and sides of buildings that are designed in a manner consistent with front or sides of building.
- C9. Screening and Landscaping:** Parking lots and loading areas with screening and landscaping.
- C10. Landscape Ordinance:** Develop ordinance that includes maintenance requirements.
- C11. Signage:** Business signage should be simple and incorporated into a building’s architecture. Such signs should serve to identify a business while contributing to Central’s attractiveness and character. The quality, size, placement, and look of signs should be considered in the overall design of the building.

Central Street: Master Plan

Section 3: Master Plan



Example of distinct, recognized storefronts at base of mixed-use building.



Example of single buildings with articulated architecture that creates appearance of multiple storefronts.



Example of storefronts should have clear glass windows to indicate an active shopping street.

D. Zoning

The following zoning changes are recommended for the Central Street corridor (See Figure 3.1 and 3.2: Zoning Recommendations). When changing the ordinance to match the Master Plan, form-based code should also be considered. Such an approach would allow a more “block-by-block” approach to defining the physical form/building envelope on Central Street, taking into account the range of existing conditions.

Potential District Changes

The following are recommended zoning changes for particular locations along Central Street:

- D1. Business District (B2):** The B2 district between Hartrey and Eastwood, except for the southern half of the Chase Bank site, should be rezoned as B1a to preserve the existing retail character and scale of these blocks. This change would lower building heights from 4 to 3 stories.
- D2. Commercial District (C1):** To encourage mixed-use developments, the C1 district south of Central on Green Bay should be rezoned as B2.
- D3. Commercial District (C2):** The C2 district at the intersection of Gross Point, Crawford and Central should be rezoned as B2 to allow residential and mixed-use developments. Buildings in this area should be set back from the curb 30 feet.
- D4: Office District (O1):** The northwest quarter of the O1 district east of Eastwood Avenue should be rezoned as B1a to allow mixed-use developments. The northeast quarter of the O1 district east of Eastwood Avenue should be rezoned as C2 to allow a greater range of retail uses.
- D5: Transitional Campus District (T1):** The part of the T1 districts south of Chancellor Street should be rezoned as C2 to allow structured parking.

Central Street: Master Plan

Section 3: Master Plan

D6: University Athletic Facilities District (U2): The southeast corner of the U2 district encompassing Ryan Field should be rezoned as B2 to allow mixed-use buildings with upper-story office.

D7: Open Space District (OS): The CTA station should be rezoned as B2 to encourage retail uses.

Potential Regulation/Form Changes

In addition to changes in the zoning districts, the following changes to the districts' bulk requirements should be considered for Central Street:

D8. Height Recommendations: Table 3.1 shows height recommendations in both feet and stories. Maximum height would be the lesser of the two.

Table 3.1: Maximum Height Recommendations

Zoning District	Current Height	Recommended Height (Stories)	Recommended Height (Feet)
R4: General Residential	35 feet/2.5 stories	2.5 stories	30 feet
R5: General Residential	50 feet/5 stories	4 stories	45 feet
O1: Office*	52 feet	5 stories	52 feet
B1a: Business	40 feet/3 stories	3 stories	35 feet
B2: Business	45 feet	4 stories	45 feet
C1: Commercial	45 feet	4 stories	45 feet
C2: Commercial	45 feet	4 stories	45 feet
U2: University Athletic Facilities	45 feet	4 stories	45 feet
T1: Transitional University	35 feet/2.5 stories	2.5 stories	35 feet
OS: Open Space	35 feet/2.5 stories	2.5 stories	35 feet

Recommended heights are shown in bold.

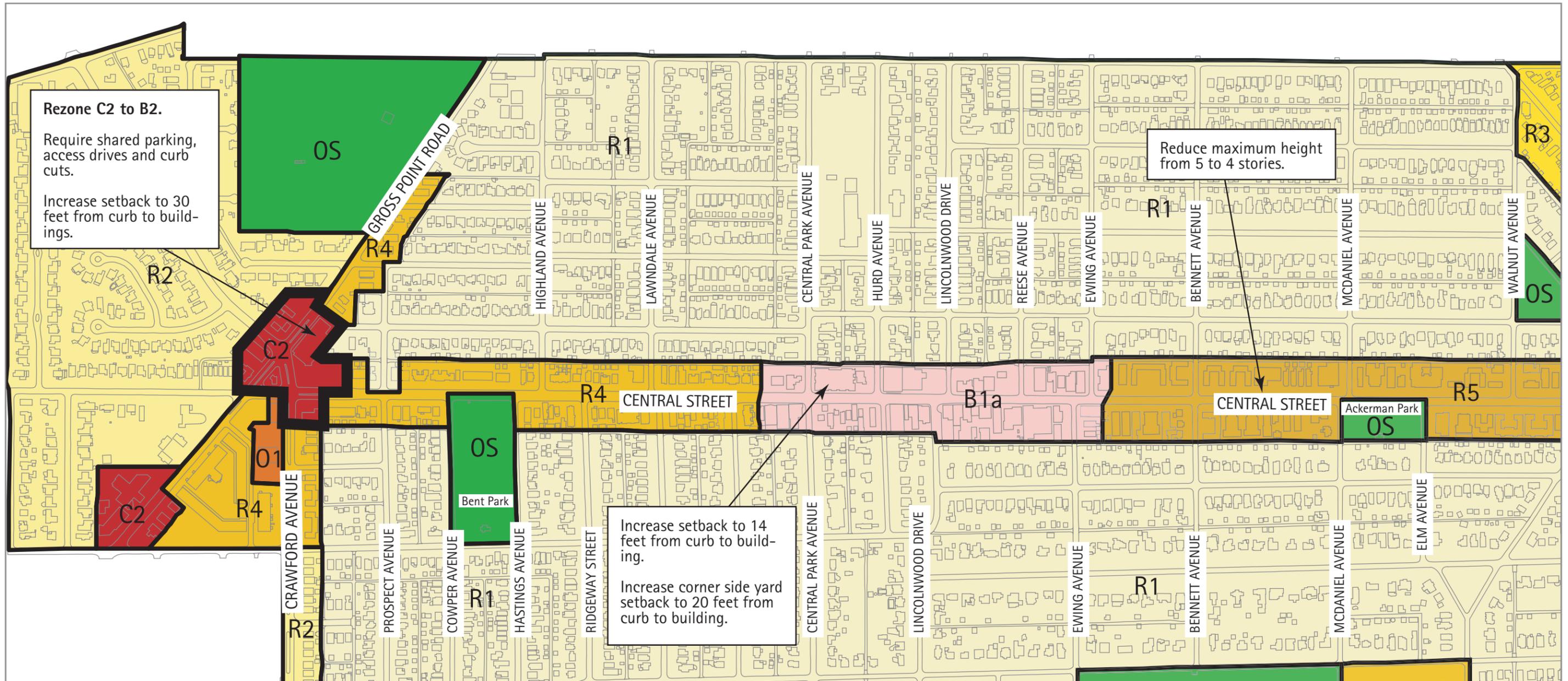
*Height limit "steps down" near adjacent residential uses.

B9. Business District (B1a), Commercial District (C1), Commercial District (C2), and Office District (O1): Buildings should be set back 14 feet from the curb to allow a wide, consistent sidewalk width. Side yard

setbacks should be 20 feet from the curb to ensure wider, landscaped parkways as a transition between retail frontages and residential side streets.

In C1 and C2, shared parking/curb cuts/access drives should be required where feasible. Parking in front of buildings should be prohibited.

D10. Commercial District (C2) - Gross Point/Crawford/Central intersections: Buildings should be set back 30 feet from the curb to provide room for wider sidewalks, additional landscaping, and improved sight lines for motorists.



City of Evanston: Central Street

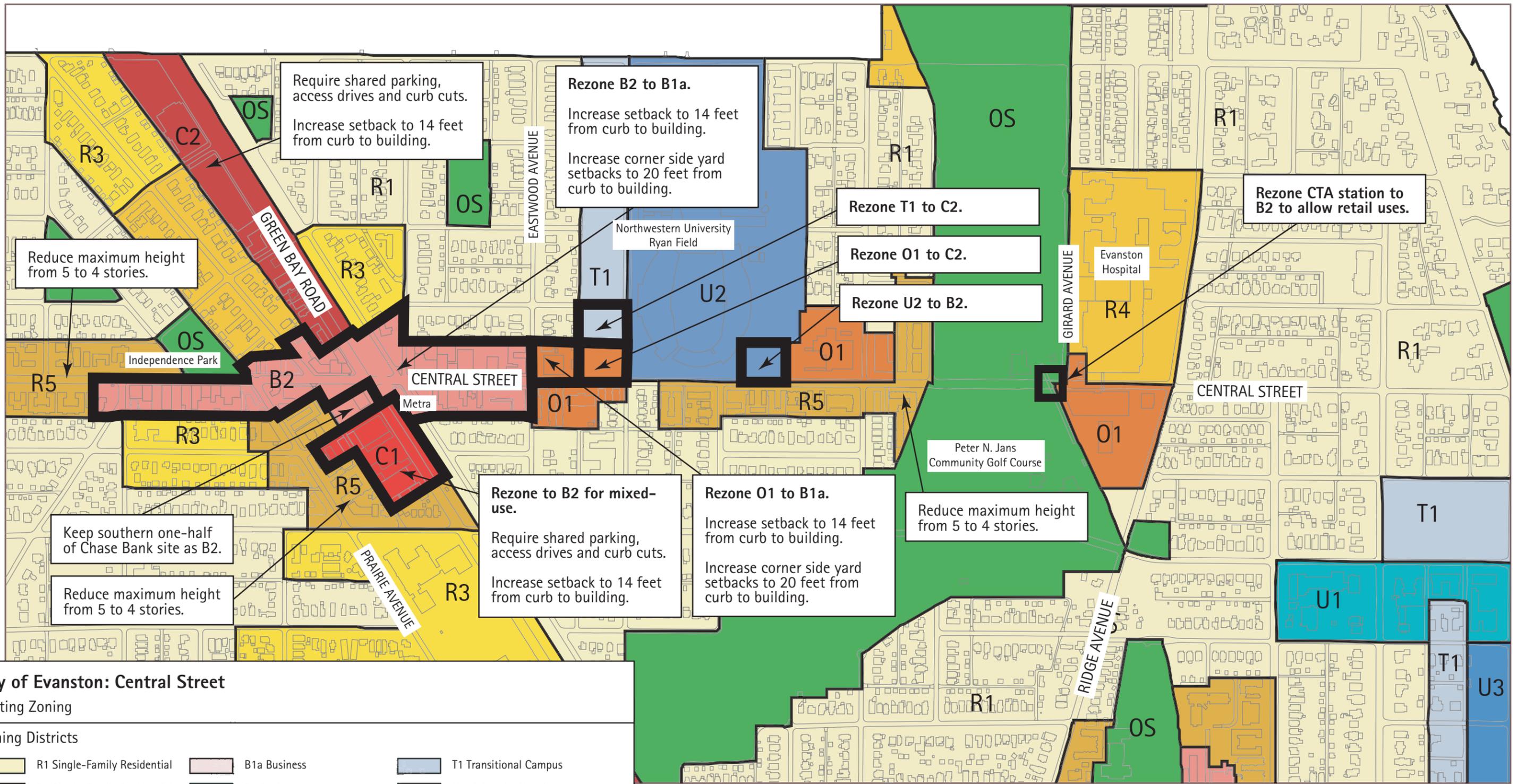
Existing Zoning

Zoning Districts

	R1 Single-Family Residential		B1a Business		T1 Transitional Campus
	R2 Single-Family Residential		B2 Business		U1 University Housing
	R3 Two-Family Residential		C1 Commercial		U2 University Housing/Parking
	R4 General Residential		C2 Commercial		U3 University Lakefront Campus
	R5 General Residential		O1 Office		OS Open Space

Central Street: Master Plan City of Evanston, Illinois

Figure 3.1: Zoning Recommendations - Gross Point Road to Hartrey Avenue



City of Evanston: Central Street

Existing Zoning

Zoning Districts

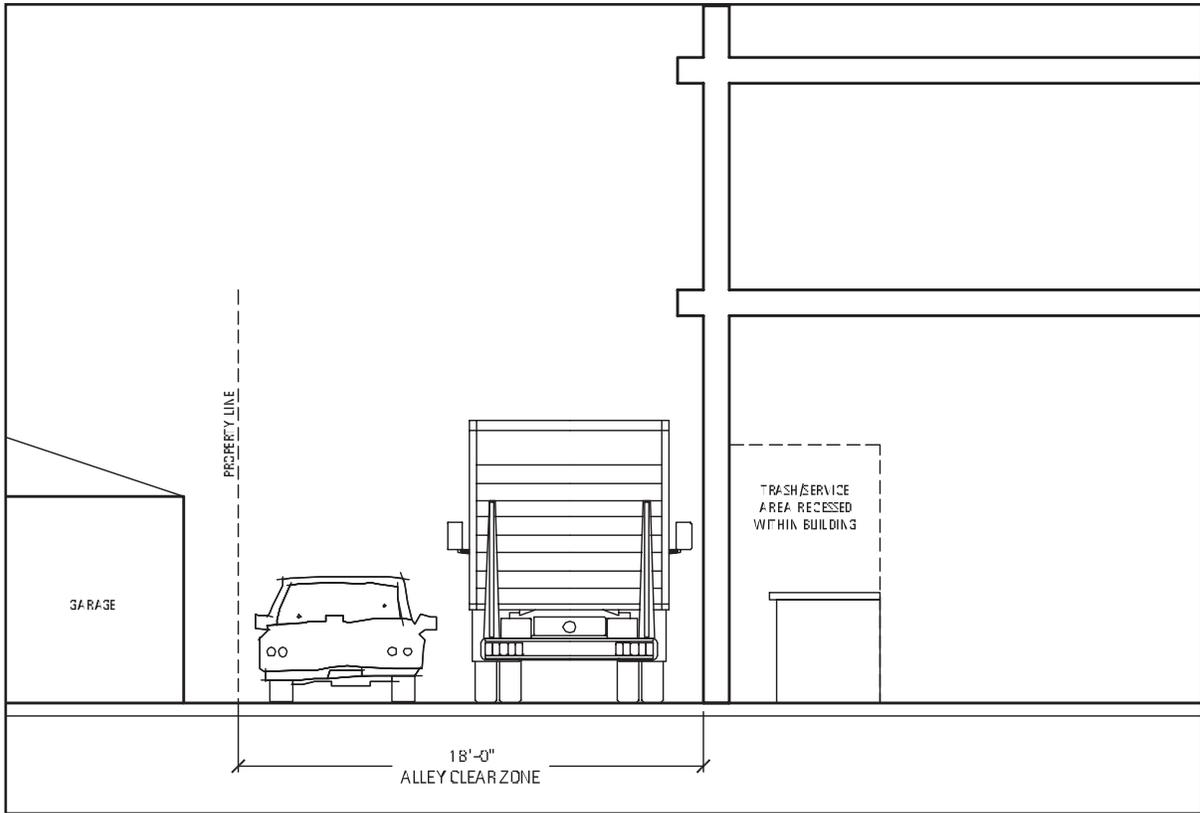
	R1 Single-Family Residential		B1a Business		T1 Transitional Campus
	R2 Single-Family Residential		B2 Business		U1 University Housing
	R3 Two-Family Residential		C1 Commercial		U2 University Housing/Parking
	R4 General Residential		C2 Commercial		U3 University Lakefront Campus
	R5 General Residential		O1 Office		OS Open Space

Central Street: Master Plan City of Evanston, Illinois

Figure 3.2: Zoning Recommendations: Hartrey Avenue to Ridge Avenue

Central Street: Master Plan

Section 3: Master Plan



Wider, unobstructed alleys will facilitate movement behind and adjacent to buildings for residents and delivery vehicles.

E. Transportation

- E1. Standard Alley Width:** A standard alley width of 18 feet should be required with direct access to new development.
- E2. Alley Obstructions:** The alley right-of-way should be kept clear of all obstructions, including utility poles, dumpsters and trash cans, and loading/service areas. Access to trash/loading/service areas as well as parking facilities within a development would be allowed directly from alleys.
- E3: Alley Right-of-Way Dedication:** Future developments should dedicate property to alley right-of-way to create an 18-foot alley width. Developers should also be required to pave the alley serving proposed developments.

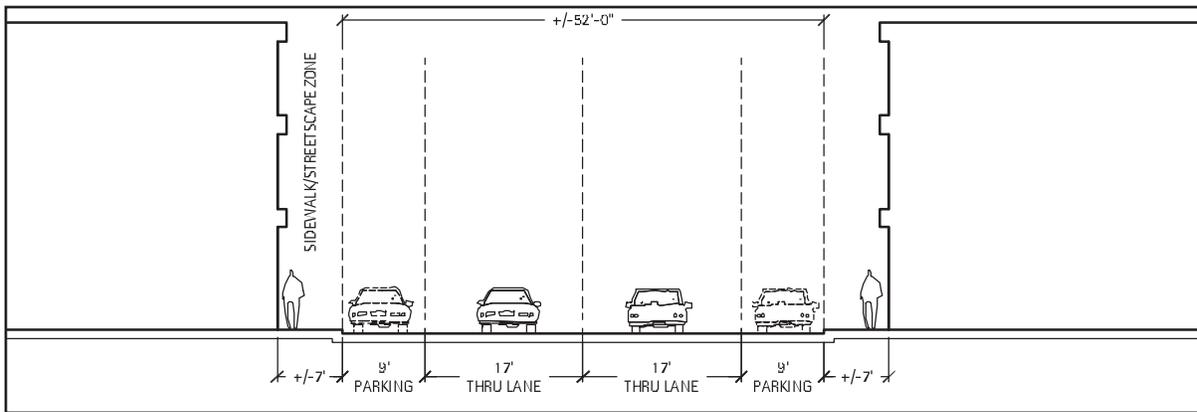


Wide, paved alley with circulation unobstructed by trash bins, poles, or other obstacles.

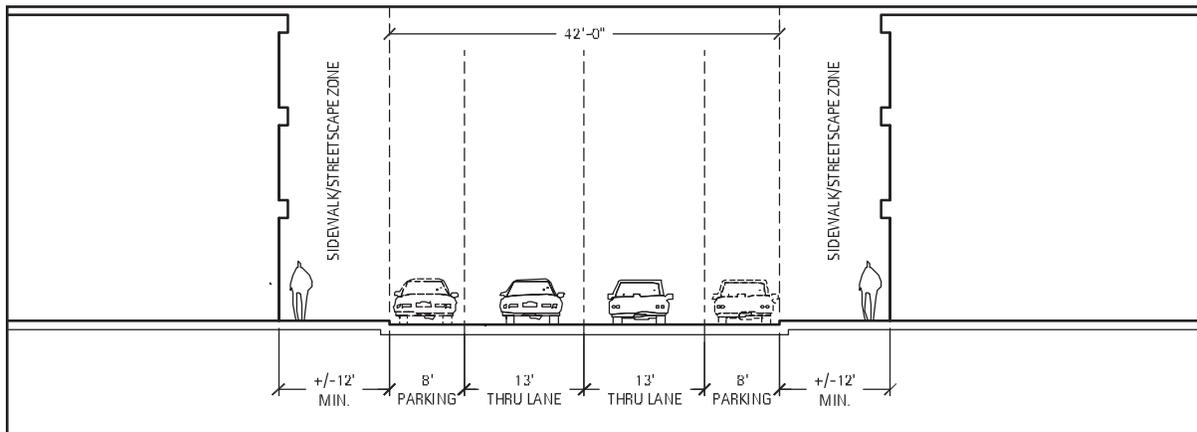
- E4. Parking Ratios:** Parking ratios should be reviewed to provide adequate spaces for new development. The current requirement for medical and dental offices of 5 spaces per 1,000 square feet may be too high for a pedestrian-oriented street with significant transit availability. The current requirement for general office of 2 spaces per 1,000 square feet may be too low and should be reviewed. Office ratios of 3 to 4 per 1,000 square feet should be considered.
- E5. Transit Drop-off Zones:** Improved drop-off zones should be provided at both the Metra and CTA stations to improve traffic flow and pedestrian safety. At the Metra station, six to seven on-street parking spaces on Poplar south of Central should be converted to a “Kiss ‘n’ Ride” area. At the CTA station, consideration should be given to removing the left-turn lane at Girard to allow vehicles to pull over in front of the station without congesting traffic flow.
- E6. Transit Routes:** Consideration should be given to expanding bus routes along Central Street operated by CTA and PACE to improve service frequency, particularly on Sundays when no service is currently

Central Street: Master Plan

Section 3: Master Plan



Existing Central Street cross-section west of Lincolnwood.



Proposed cross-section west of Lincolnwood, with narrower travel lanes and wider sidewalks.

provided. In addition, benches and shelters should be added to existing bus stops.

E6. Street Width: Central Street between Crawford Avenue and Lincolnwood Drive should be narrowed to either 40 or 42 feet, where feasible. A width of 42 feet would allow for bike route markings, which encourage automobiles and bicyclists to share a wider travel lane. The narrower street width also would allow for an additional 5 feet of sidewalk/streetscape zone on each side of the street.

E7. Bike Routes: Although cyclists should be encouraged to use nearby east-west residential streets, the addition of bike routes to Central Street should be considered where the street width allows.

- E8. Crosswalks:** Crosswalks along the corridor should be improved and standardized. Consistent signage, markings, and lighting should be used to effectively identify crosswalk locations to both vehicles and pedestrians, particularly at mid-block locations. Pedestrian signals should be enhanced, including countdown timers.
- E9. Bump-outs:** Sidewalk bump-outs should be installed where appropriate to ensure shorter, safer crossings for pedestrians while still allowing efficient traffic flow.

F. Streetscape

The following are recommendations for streetscape improvements. Also see Central Street: Streetscape Study for more information on streetscape conditions and design concepts.

- F1. Pavement:** Improve the paved surface for pedestrians, bicycles and vehicles.
- F2. Street Furniture:** Upgrade the quality and increase the amount of street furniture such as benches, trash receptacles, moveable planters, bike racks, and tree grates.
- F3. Street Trees:** Establish a consistent, well-maintained street tree program with street trees spaced 25 to 30 feet, and improve landscaping in adjacent open spaces.
- F4. Lighting:** Improve lighting conditions throughout all commercial districts and replace any light bases or poles that are in disrepair.
- F5. Crosswalks:** Enhance pedestrian crosswalks with new paving or markings, improved lighting and consistent signage.
- F6. Landscaping:** Screen and buffer surface parking lots bordering the street with landscaping and/or ornamental fences.
- F7. Sidewalk Width:** Establish a standard sidewalk width within commercial areas of 14 feet and within residential areas of 5 feet, where feasible.
- F8. Street Width:** Decrease the street width west of Lincolnwood to approximately 42 feet to provide additional sidewalks and pedestrian refuge.

Central Street: Master Plan

Section 3: Master Plan



Example of pedestrian-friendly streetscape improvements.



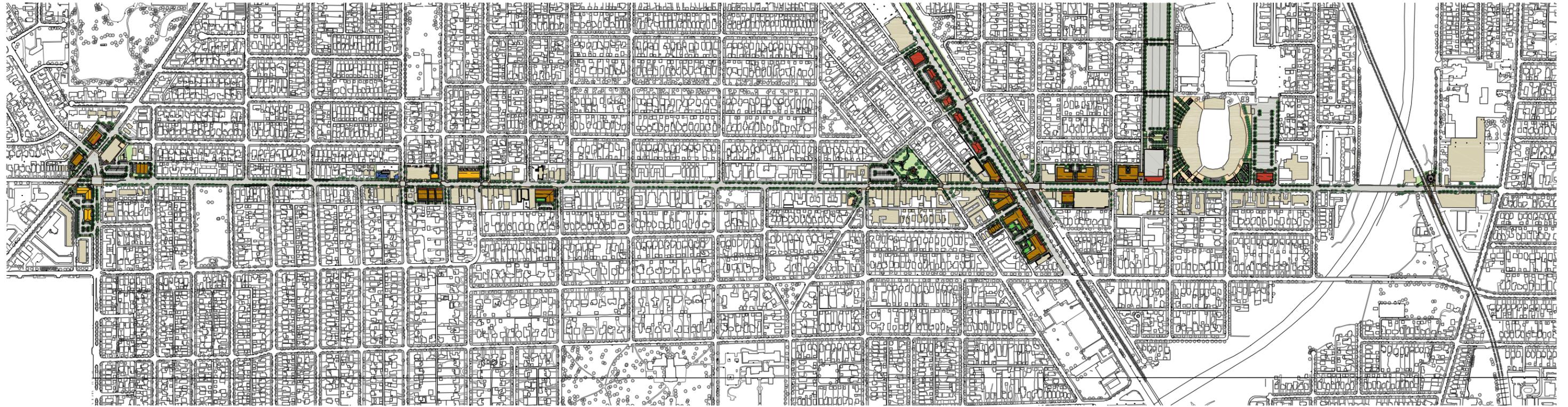
Corner plaza/gateway example.

Development Concepts

The Master Plan envisions coordinated and balanced preservation and redevelopment of Central Street that allows for significant retail, office, residential, and open space opportunities, while maintaining the unique character and quality-of-life that currently exists (see Figure 3.3).

The site designs illustrated in the Master Plan indicate conceptual development densities, site plans, landscape/streetscape, and parking layouts. Actual building locations, heights and densities, as well as site designs, will vary as property owners, businesses, and developers generate more specific site plans. Each development will need to address parking needs within the context of zoning requirements, parking availability, shared parking potential, and linked parking lots.

The new development shown in the Master Plan will be largely driven by the private sector. While the City can encourage and facilitate property improvements and new development by investments in infrastructure, the Plan envisions property owners improving or redeveloping their sites or cooperating to create larger, more viable redevelopment opportunities. Ideally, many of the businesses on sites suggested for redevelopment will find new locations in the corridor and continue to make Central Street a great place to live, work, and shop.



Central Street: Master Plan City of Evanston, Illinois

Figure 3.3: Illustrative Plan

LAKOTA
THE LAKOTA GROUP INC



GEVALI HAMILTON
ARCHITECTS



0' 75' 150'

July 2007

Western Gateway

The intersection of Crawford, Gross Point and Central is an important “gateway” to Central Street. The triangular island in the center of these streets has high visibility and could serve as a prominent gateway green space with signage and/or public art that would announce motorists’ arrival to the City and to Central Street.

Framing the intersection with buildings as new development occurs and providing ample, 30-foot setbacks with a green, landscaped character also would improve the appearance of this gateway intersection. Improved sidewalks, wider parkways, pedestrian-scale lighting, consistent street furniture, and additional street trees would improve the attractiveness and pedestrian-friendliness of the intersection.

For the blocks abutting the intersection of Gross Point, Crawford and Central, the Master Plan shows (see Figure 3.4):

- **Gateway Green Space:** The Plan envisions this location as an opportunity for improved landscaping, public art, and directional signage. Because of its location between three streets, the space is intended as a visual gateway element only and not for use by residents as a park or seating area.
- **CVS Pharmacy Site:** This large, prominent site is shown with enhanced landscaping and streetscape. Its inefficient parking lot is reconfigured to create room for a landscape buffer and street trees along the perimeter. If the existing CVS pharmacy is redeveloped, the Plan shows how new retail buildings could frame the corner and have larger setbacks to improve its appearance and provide better sight lines for motorists and pedestrians. Parking would be accommodated to the rear of the buildings with screening



Landscaped/screened parking lot edges.

and buffering along the residential uses to the east. New rowhomes are shown east of the site.

- **Southwest Corner Central/Crawford:** Existing office buildings are shown as being redeveloped into modern office buildings. Parking would also be accommodated in the rear and access would be provided through shared curb cuts.
- **Northeast Corner Crawford/Gross Point:** A mixed-use development is envisioned on the Citgo/Sarkis site. The Plan shows retail space on the first floor with condominiums or office space above, as well as an adjacent side parking lot.
- **Northwest Corner Crawford/Gross Point** The Plan shows a condominium building on the northwest corner of Crawford and Gross Point and west of the CVS site.



Alternate - CVS Site



Central Street: Master Plan

Section 3: Master Plan

Central Park to Ewing Commercial Area

Although this area has a large number of professional, financial and personal services businesses, it also has active pedestrian-oriented retail serving nearby neighborhoods. A healthy balance of retail and service uses should continue to flourish in this area and will be aided by an improved pedestrian environment. Narrowing extra-wide travel lanes west of Lincolnwood will allow for wider sidewalks and additional streetscape elements, such as street trees, benches, and trash cans.

For the blocks along Central between Central Park and Ewing, the Master Plan shows (see Figure 3.5):

- **Mixed-Use Development:** The Plan shows several locations for mixed-use buildings with retail uses on the ground floor and either upper-story office or residential uses. Infill development is proposed to create more of a building “streetwall” that minimizes curb cuts and enhances the pedestrian shopping environment.
- **Pedestrian Improvements:** The Plan shows Central Street narrowed west of Lincolnwood to provide space for expanded sidewalks/streetscapes.
- **Mitchell Museum:** The Mitchell Museum of the American Indian is considering site improvements and possibly an expansion of its facility to increase activity and recognition. The Plan shows expansion to the west side of the structure and a concept for a gateway/entrance that could possibly include a gift shop component at the corner of Central and Central Park.



Central Park to Ewing Commercial Area facade improvement (building on left) and redevelopment concept (on right).



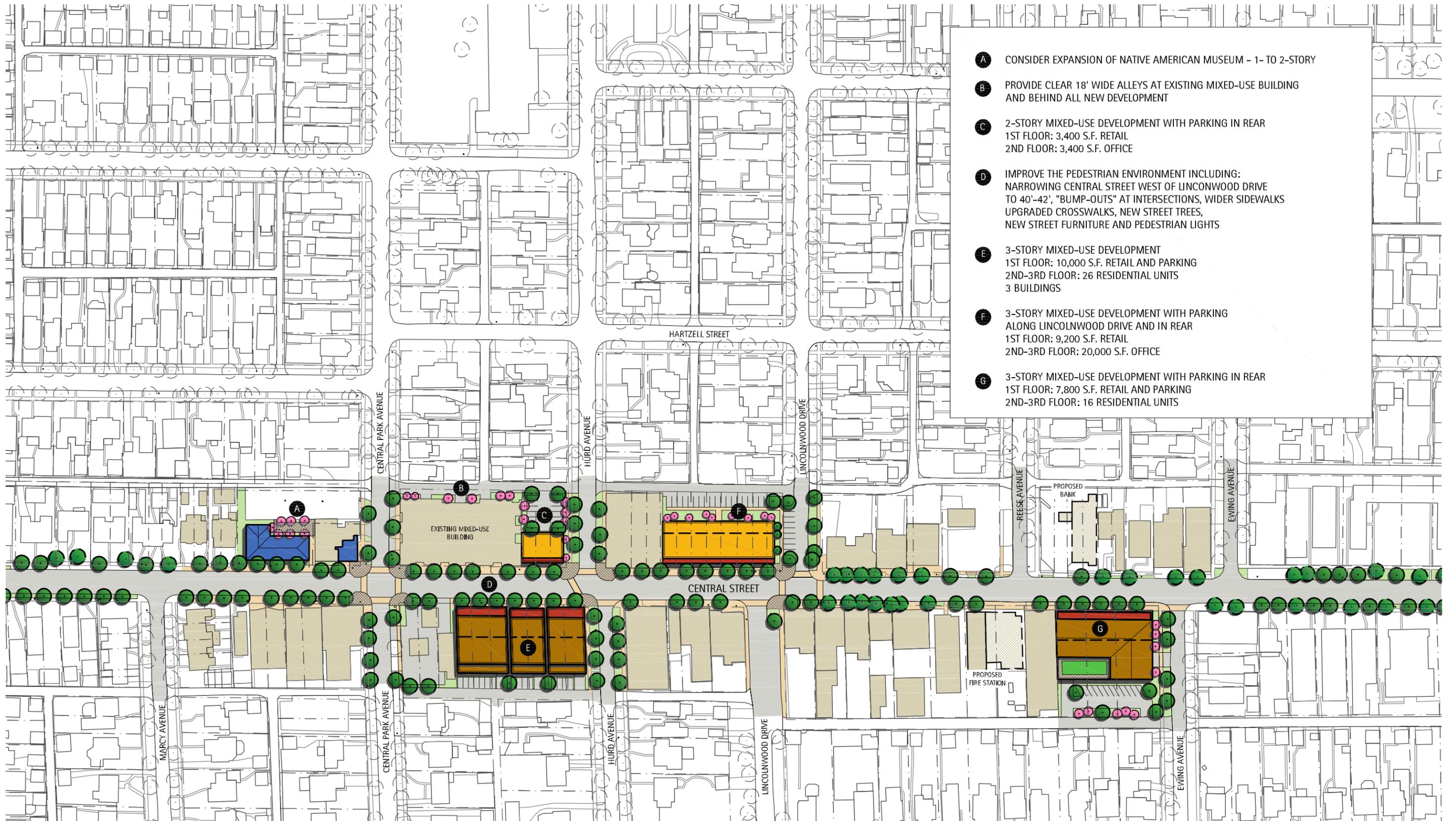
Rowhomes with landscaped front yards and attractive, articulated facades.



Mixed-use building with retail on first floor and residential above.



Condominium building with quality materials and green setback.



- A** CONSIDER EXPANSION OF NATIVE AMERICAN MUSEUM - 1- TO 2-STORY
- B** PROVIDE CLEAR 18' WIDE ALLEYS AT EXISTING MIXED-USE BUILDING AND BEHIND ALL NEW DEVELOPMENT
- C** 2-STORY MIXED-USE DEVELOPMENT WITH PARKING IN REAR
1ST FLOOR: 3,400 S.F. RETAIL
2ND FLOOR: 3,400 S.F. OFFICE
- D** IMPROVE THE PEDESTRIAN ENVIRONMENT INCLUDING:
NARROWING CENTRAL STREET WEST OF LINCONWOOD DRIVE TO 40'-42', "BUMP-OUTS" AT INTERSECTIONS, WIDER SIDEWALKS
UPGRADED CROSSWALKS, NEW STREET TREES, NEW STREET FURNITURE AND PEDESTRIAN LIGHTS
- E** 3-STORY MIXED-USE DEVELOPMENT
1ST FLOOR: 10,000 S.F. RETAIL AND PARKING
2ND-3RD FLOOR: 26 RESIDENTIAL UNITS
3 BUILDINGS
- F** 3-STORY MIXED-USE DEVELOPMENT WITH PARKING ALONG LINCOLNWOOD DRIVE AND IN REAR
1ST FLOOR: 9,200 S.F. RETAIL
2ND-3RD FLOOR: 20,000 S.F. OFFICE
- G** 3-STORY MIXED-USE DEVELOPMENT WITH PARKING IN REAR
1ST FLOOR: 7,800 S.F. RETAIL AND PARKING
2ND-3RD FLOOR: 16 RESIDENTIAL UNITS

Central Street: Master Plan City of Evanston, Illinois

Figure 3.5: Preliminary Plan - Central Park to Ewing Commercial Area

Hartrey to Poplar Commercial Area

The area's main commercial core between Hartrey and Green Bay has a successful mix of specialty shops, restaurants, and services, as well as important open space, Independence Park. Residents appreciate the character and scale of these retail blocks, with their mix of 1-, 2-, and 3-story buildings. The Plan recommends that this scale be maintained.

Recommendations shown for Independence Park will allow for more activity by a wider age range while improving the physical attractiveness of the entire area. The park's function as a "Village Green," central amenity and festival/event location will grow even stronger.

Streetscape improvements will add to the street's charm, improve pedestrian flow, provide additional seating pockets and direct pedestrians to more prominently and consistently marked crosswalks.

The Master Plan recommends the reconfiguration of the public parking lot and Stewart Avenue intersection, as well as the addition of short-term parking space for "quick" trips and some long-term spaces near restaurants. It is also recommended that the Chase Branch parking lot be formally used for shared parking for area shoppers and dining patrons as well as bank customers.



Existing character along Central between Hartrey and Prairie.

The Plan shows (see Figure 3.6):

- **Enhanced Open Space:** The Plan shows enhanced landscaping, fencing along the alley, and lighting in Independence Park, along with seating pockets, gardens, and possible water features.
- **Green Bay development:** The Plan envisions mixed-used development along Green Bay south of Central with ground floor retail and upper story office and residential uses.
- **Parking Lot:** Reconfiguring the public parking lot at the northwest corner of Central and Stewart would provide for a more efficient parking layout, add a few additional spaces, and eliminate the need for a driveway entrance on Central.
- **Stewart Avenue Realignment:** The Plan shows how Stewart Avenue could be realigned to a 90-degree intersection with Central to improve sight lines and traffic flow. Such a change would also increase the size of Independence Park.
- **Southwest Corner Hartrey/Central:** The Plan shows a future adaptive reuse of the building at the southwest corner of Hartrey and Central as a restaurant with outdoor dining, retail, office or a civic use.

Central Street: Master Plan

Section 3: Master Plan



Mixed-use building with attractive, articulated architecture.



Redevelopment concepts for the Hartrey to Poplar Commercial Area.



Mixed-use building with varied heights and rooflines.



Central Street: Master Plan City of Evanston, Illinois

Figure 3.6: Preliminary Plan - Hartrey to Poplar Commercial Area



Central Street: Master Plan

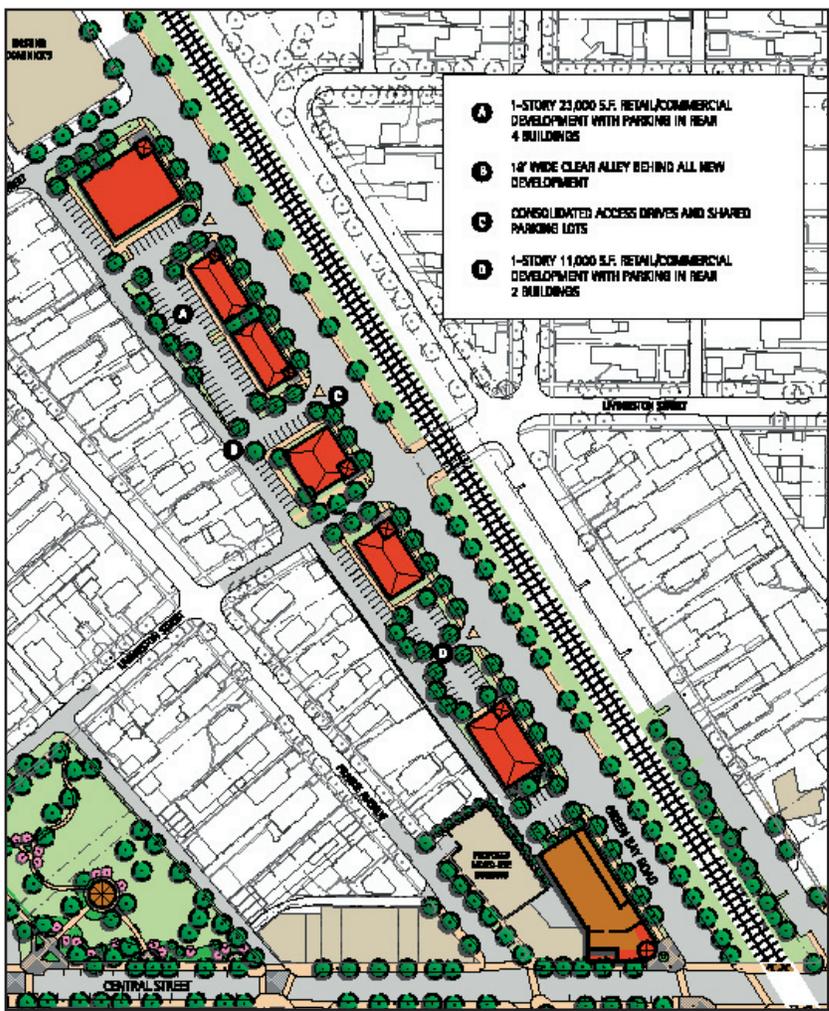
Section 3: Master Plan

Green Bay Road (North)

For the auto-oriented commercial blocks located along Green Bay Road between Central and Jenks, the Plan shows (see Figure 3.7):

- **Pedestrian-friendly:** Retail development in this area would remain auto-oriented and low-scale, but the placement of buildings and position of entrances and fenestration would result in more pedestrian-friendly structures. Wide sidewalk/streetscape zones would also provide space for street trees and enhance the attractiveness of the corridor.
- **Parking:** The Plan envisions that the parking for new retail uses will be placed to the rear and the sides of the buildings. Where feasible, parking lots, curb cuts and access drives should be shared.

Figure 3.7: Preliminary Plan - Green Bay Road (North) - DRAFT

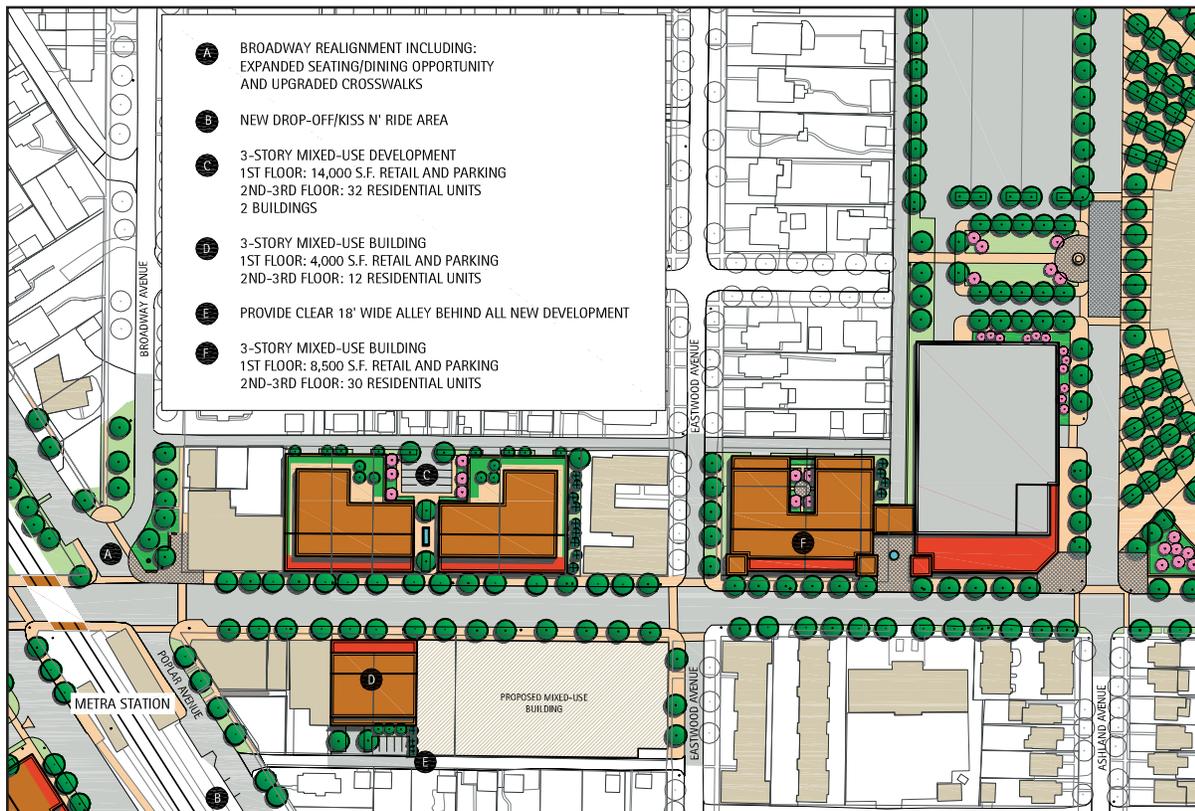


East of Metra

This area is targeted for infill retail/service development, including renovation of existing buildings to bring additional retail vitality to the area. For the blocks along Central between Poplar and Eastwood, the Master Plan shows (see Figure 3.8):

- Mixed-use Development:** As smaller, underutilized properties are developed, the Master Plan envisions buildings with ground floor retail and restaurant space, as well as upper-story residential units or office space. This will create a more continuous and active “streetwall.” Shared structured parking at the Ryan Field site would increase parking opportunities for shoppers, employees, and commuters within these blocks, further increasing retail viability.
- Poplar/Broadway Intersection:** A reconfiguration of the complicated and busy intersection is recommended. It would eliminate one lane, channel traffic to a better alignment with Poplar, and increase sidewalk width for the many commuters who walk through this intersection.

Figure 3.8: Preliminary Plan - East of Metra



Ryan Field

The Northwestern University athletic facilities, which include both its football stadium and basketball facility, are a key landmark along east Central Street and an important regional and national attraction on game days. Its large but sparsely landscaped and deteriorated parking lots are utilized daily by university staff and students, hospital employees, and commuters. In the short term, improvements to paving, drainage and landscaping would improve the attractiveness of the large lots. The Plan also envisions changes that would provide a more attractive and active environment around this world-class institution that is more integrated into the daily life of Central Street.



Ryan Field and part of its extensive parking lot.

Landscaped areas and plazas in front of the stadium along the Central Street frontage would present a photogenic “front door” for the stadium. A parking garage with retail frontage along Central would provide an opportunity for the University, Evanston Hospital (ENH), Metra and CTA commuters, shoppers and City residents to share parking in this strategic location. On game days, the retail spaces would be active as fans pick up souvenirs, dine, or, if they don’t have a ticket, watch the big game at a nearby sports bar, possibly with cafe seating outside or on a rooftop deck. On other days, the retail/restaurant locations would fit in with rest of the Central Street businesses and would serve the many people using the structured parking and adjacent lots.

Behind the parking garage and in front of the stadium’s grand entry point, the Plan envisions a green plaza that would provide tailgating/picnicking opportunities, a location for photographs or television shots of the stadium’s main facade, and a spot for festival or special events activities. Landscaped pedestrian pathways also would provide routes for neighborhood residents to walk through the lot directly to the stadium. An ample landscape buffer and fencing would provide separation between the parking lot and adjacent residences.

For the area surrounding and encompassing Ryan Field, the Master Plan shows (see Figure 3.9):

- **Physical Changes:** To change the appearance of the lots, the Plan shows significant new landscaping and streetscape improvements. The parking lot would have



Example of structured parking with attractive facade located behind a restaurant.

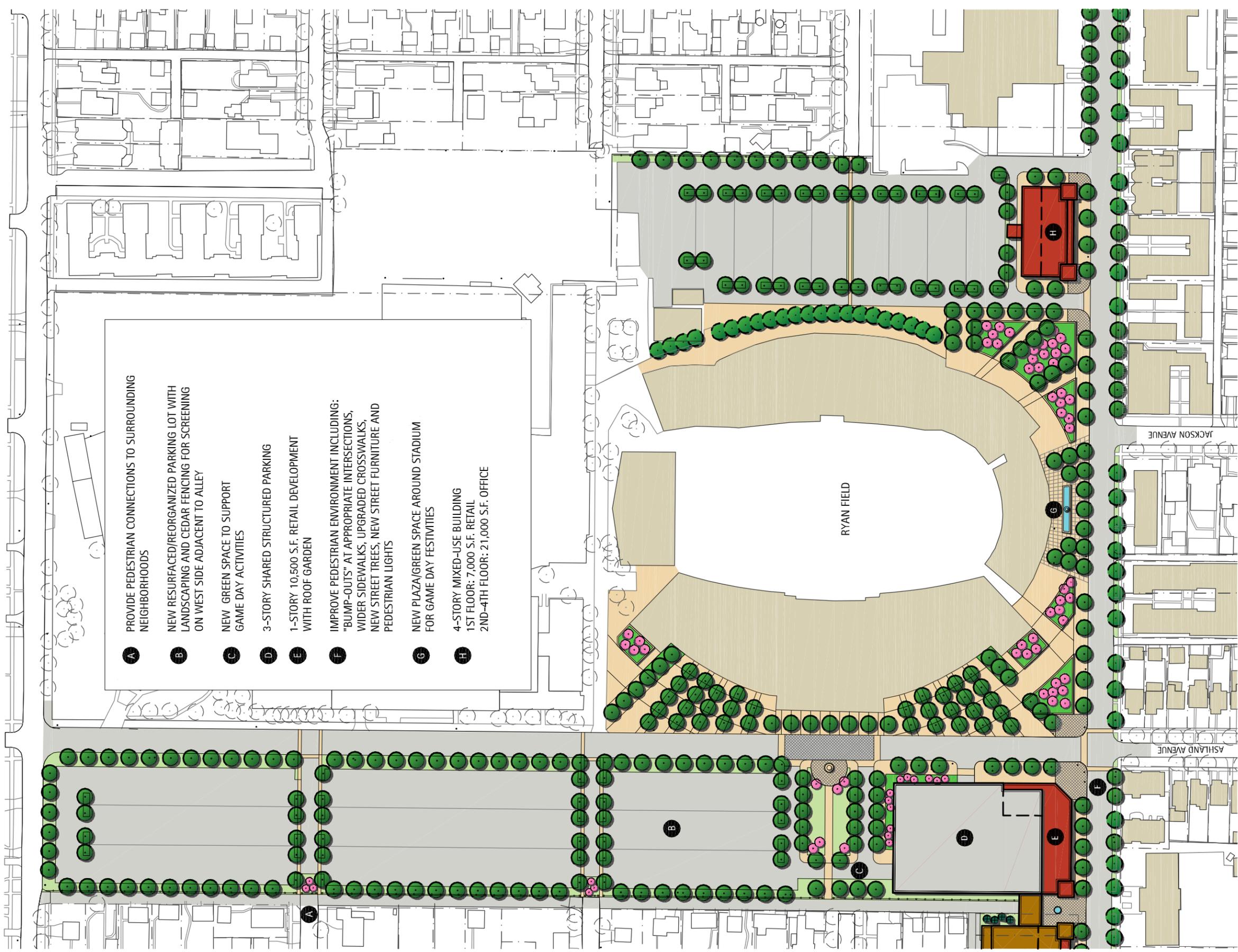
landscape “islands” with trees and shrubs to break-up the visual monotony of the paving. Improved stormwater management would prevent water from pooling on the site.

- **Structured Parking/Retail Space:** Structured parking is incorporated into the west parking lot at the northwest corner of Central and Ashland. Such a facility would increase the number of parking spaces near Central Street and transit stations, which are approximately a quarter mile away. Structured parking in this location could provide additional parking for stadium functions, hospital and university employees, commuters and shoppers.

Retail space is envisioned in front of the parking facility to screen the structure and create additional shopping/dining activity in the area.



A structured parking development and redevelopment concepts for the Ryan Field area.



Central Street: Master Plan

Section 3: Master Plan

Eastern Gateway

Although separated from commercial and residential areas to the west by the North Shore Channel, this area also serves as an important gateway to Central Street either by road or public transit. The Plan shows a renovated transit plaza at the northwest corner of Girard and Central that would create an attractive place for people to wait for buses and trains, or to enter the golf course “starter shack” (see Figure 3.10). Streetscape improvements and reinvigorated retail at the CTA station would also improve the attractiveness and vitality of the area.

Figure 3.10: Preliminary Plan - Eastern Gateway - DRAFT

